

NAPA PIPE

# Development Plan

*Volume 1*

*Updated September 5, 2014*



VOLUME 1

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VOLUME 2

EXHIBIT A

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EXHIBIT B

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EXHIBIT C

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EXHIBIT D

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I.1 INTRODUCTION AND RELATIONSHIP TO OTHER GOVERNING DOCUMENTS

Figure I.1.a - GOVERNING DOCUMENTS RELATIONSHIP



This Development Plan is one of several documents that will guide development of the Napa Pipe Project area. Two other documents are also important to successful implementation of the plan: the Napa Pipe Project Design Guidelines and the Development Agreement. These three documents have been drafted and approved concurrently with this Development Plan.

**County Approvals**

The Napa Pipe Project has undergone a multi-year design and review process by Napa County. On June 4, 2013, the Napa County Board of Supervisors approved a General Plan Amendment and Zoning Ordinance for the Property that would allow development of a 63-acre mixed-use neighborhood on the waterfront portion of the Property, and would allow commercial uses on the 91-acre eastern parcel, including a 154,000 sf Costco-type wholesale retailer, up to 90,000 gross square feet of office use and up to 75,000 gross square feet of warehouse/R&D use. Napa County also approved a Final EIR for the project, and adopted CEQA findings and mitigation measures connected to the General Plan Amendment and Zoning Ordinance and map amendments.

**Development Plan**

County approvals mandated that all development within the Napa Pipe Project “shall be in accordance with an approved development plan.” This Development Plan was approved by the county in a manner similar to that of a specific plan. The Development Plan provides a detailed site plan with associated land uses and public/private improvements. It also contains sufficient detail for the intensity of uses, schematic details for public improvements including open space and circulation, and phasing of the development over time.

**Design Guidelines**

County approvals also mandated “All development and uses within the NP districts shall be in accordance with approved design guidelines.” Therefore, all private development in the Napa Pipe Zoning Districts shall be subject to the Napa Pipe Design Guidelines, which is a companion document to this Development Plan. The Design Guidelines establish the look and character of the Napa Pipe Project, and include sections for form-based code (setbacks, heights, parking, etc.), architectural styles, and additional building standards and criteria (details such as parking layouts, building entries, massing, and many others).

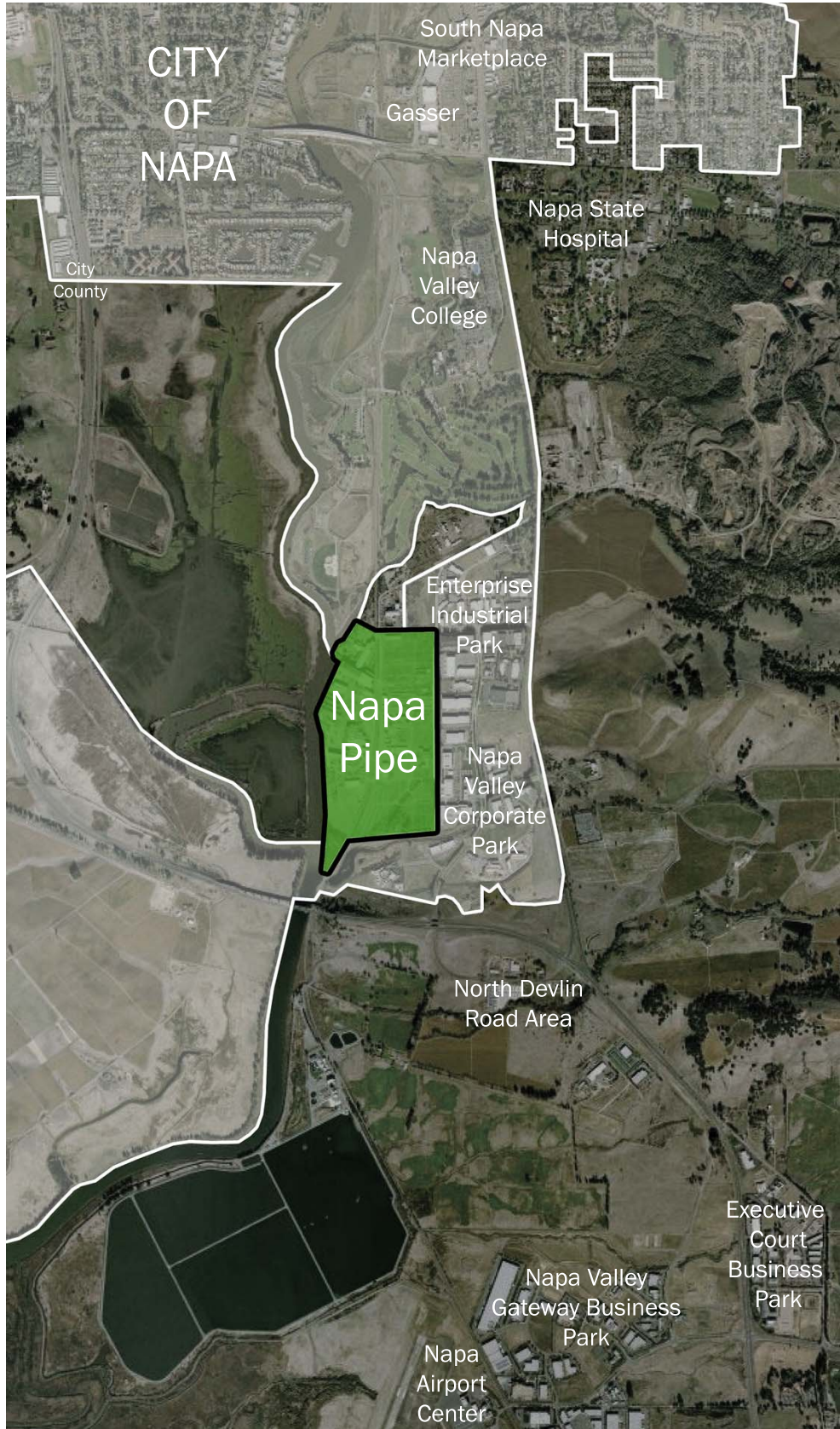
**Development Agreement**

The Development Agreement between County and Developer (ultimately accepted by the City when annexation proceedings with LAFCO take place) memorializes the rights, obligations, and contractual matters by establishing the terms and conditions for development of the site, including phasing, installation of infrastructure, housing requirements, fees, financing, approval processes and procedures, etc.



I.2 NAPA PIPE VINCINITY MAP

Figure I.2.a - SITE BOUNDARY EXHIBIT



Site Description

The Project Site is located approximately ¼-mile west of Highway 221/Soscol Avenue, and ¼-mile north of Highway 29. The Napa County Airport is approximately 2 miles south of the site. Downtown Napa is approximately 3 miles to the north. Primary access to the site is via Kaiser Road from 221/ Soscol Avenue. During the planning and entitlement phases of this project, the site was located within Napa County, directly adjacent to City of Napa. It is anticipated that the project ultimately will be incorporated in to the City subject to a Rural Urban Limit (RUL) extension approved by Napa voters.

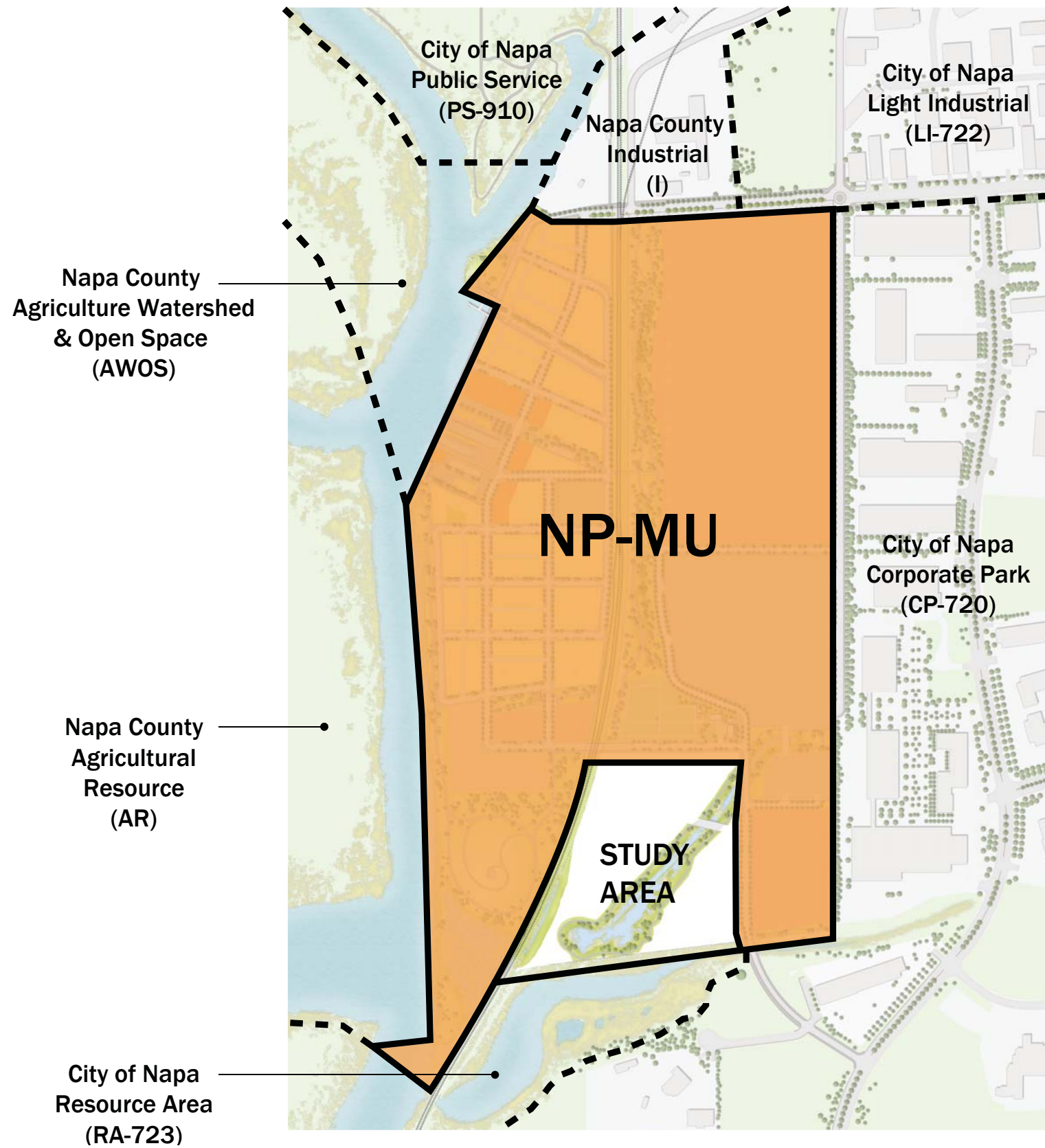
The site is a 154-acre former industrial site located at 1025 Kaiser Road. During the planning and entitlement period, the site was comprised of just 2 parcels: APN 046-412-005 (+/- 63 acres) and APN 046-400-030 (+/- 91 acres). These two parcels are divided by the Union Pacific Railroad (“UPRR”) right-of-way, which runs through the site in a north-south direction.

The site is bounded by the Napa River to the west, industrial uses to the north, and adjacent to the Napa Valley Commons to the east and south. Bedford Slough also lies to the South, and associated wetlands are located both on and adjacent to the site.



I.3 GENERAL PLAN LAND USE DESIGNATIONS

Figure I.3.a - GENERAL PLAN LAND USE DIAGRAM



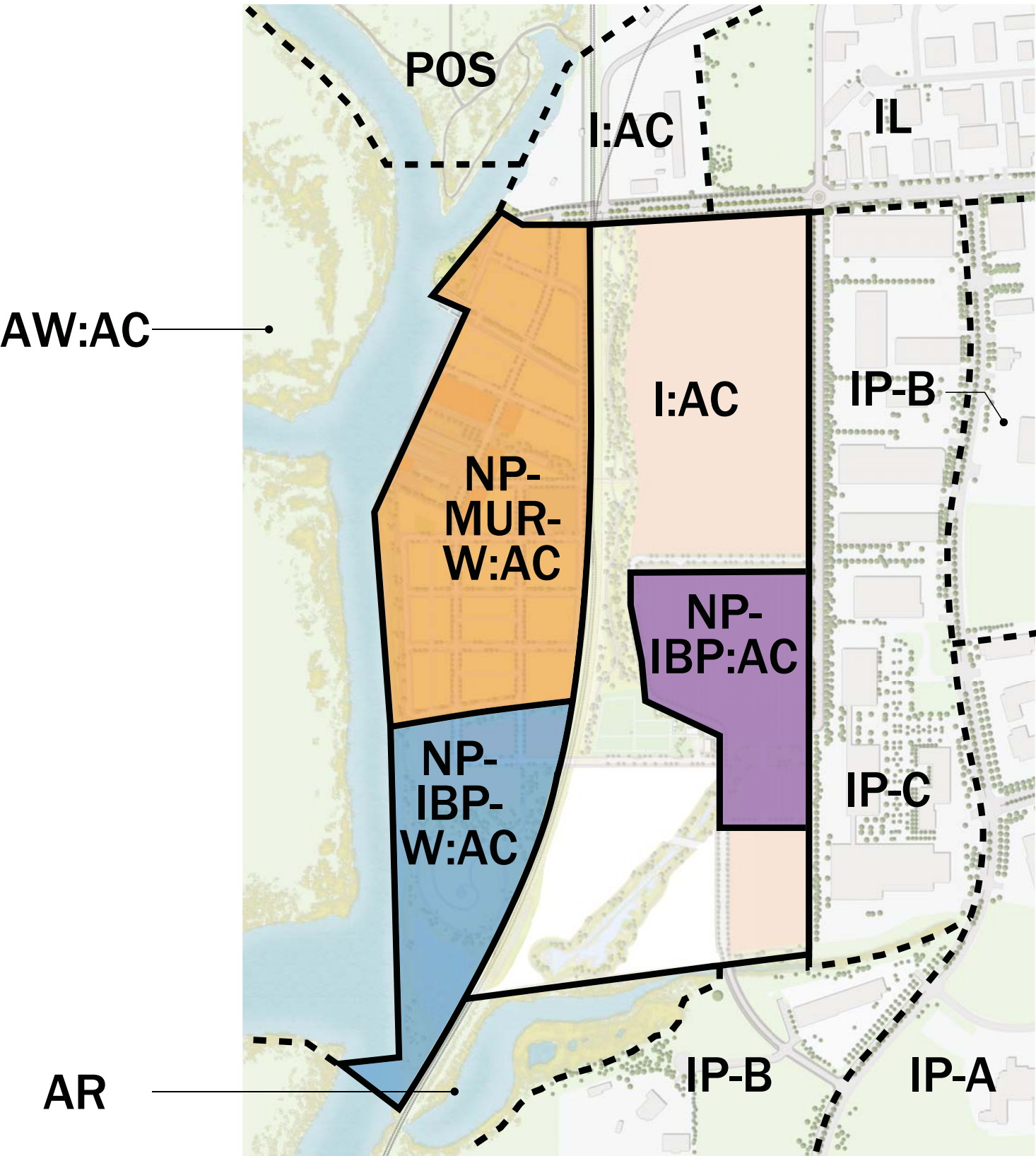
The following General Plan land use designations apply to the site.

**Napa Pipe Mixed Use (NP-MU)**  
“The designation provides for flexibility in the development of land, allowing either industrial, or commercial and residential uses. This designation is intended to be applied only to the Napa Pipe site in the unincorporated area south of the City of Napa where sufficient infrastructure may be available or readily constructed to support this type of development.”

**Study Area**  
“The Study-Area designation is intended to be applied only to the portion of the Napa Pipe Project site that is not designated Napa Pipe Mixed Use.” This Study Area is within the Napa Pipe Project area. The designation allows industrial uses to continue pursuant to existing zoning, but also signals the need for future site or area-specific planning to assess future uses in the area.

Figure I.4.a - ZONING MAP

I.4 APPLICABLE ZONING REGULATIONS AND ZONING MAP



Zoning for the project consists of four districts:

**NP-MUR-W: AC (Napa Pipe – Mixed Use Residential – Waterfront: Airport Compatibility)**

This mixed use designation overlays the western portion of the project area, between the Napa River and the railroad tracks, encompassing many of the project’s core components.

The MUR district is characterized by a mix of housing types, neighborhood services, such as retail and restaurant, common use/open space including open space, parks, and community facilities. The intent of this district is to enable a vibrant, mixed use neighborhood oriented towards the Napa River.

**NP-IBP-W:AC (Napa Pipe – Industrial Business Park – Waterfront: Airport Compatibility)**

The NP-IBP-W is also adjacent to the Napa River, but residential uses are not permitted in this area. From the originating ordinance: The purpose of the IPB-W district is to provide for office, hotel, and similar uses. Allowed uses in the IBP district are intended to be compatible with each other and with the adjoining non-industrial areas. Land uses in the IBP district are subject to standards to ensure harmonious, unified, and cohesive development that is oriented towards the Napa River.

**NP-IBP:AC (Napa Pipe – Industrial Business Park: Airport Compatibility)**

The NP-IBP district has been placed upon the future site of the membership warehouse club retail use.

**I:AC (Industrial: Airport Compatibility )**

The I designation is retained for most areas to the east of the railroad tracks (except for the NP-IBP area described above).

All zoning districts described above are also subject to the Airport Compatibility (AC) overlay district controls, given its proximity to the Napa County Airport.



II.1 LAND USE PLAN

Figure II.1.a - LAND USE PLAN (Term Sheet Exhibit 2)



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III.1 USES OF THE PROPERTY

TABLE III.1.a - LAND USE SUMMARY

Land Use	Acres (approx.)	Max. Intensity of Use (units/commercial space)	Intended Blocks
Parks and Open Space	34.4		P1-P9
Seniors Housing	1.8	150* units (225 beds)	10
Ground-floor Retail (w/ Residential Above)	2.1	40,000sf	11,13,15,16
Hotel & Accessory Uses	3.1	150 rooms / 200,000sf	E
Office	1.6	10,000sf	C & D
Office, R&D, Warehouse and/or Light Industrial	34.7	90,000sf Office and 75,000sf warehouse/R&D	A & G
General Wholesale Sales Commercial Activities	17.8	154,000sf	F
Residential	19	945 units (with density bonus allowance, uncluding up to 190 "affordable" units on lots 4, 17 and 19 with the moderate income units dispersed throughout)	1, 2, 3, 4, 5, 6, 7, 8, 9, 11, 12, 13, 15, 16, 17, 18, 19, 20, 21, 22
Circulation	21		n/a
Study Area	19.2		B
Total	154.3	-	-

\* Note: CCRC is not included within total unit count

The Napa Pipe Project shall include the following uses.

*Areas West of the Railroad Tracks*

**Residential Units**

The project will accommodate up to 945 residential units (which includes the State-allowed density bonus) at a gross density of 20 dwelling units per gross acre. Residential buildings will consist of a variety of low to midrise buildings and shall be consistent with the Napa Pipe Project Design Guidelines. Market-rate housing is intended to be located on the following blocks: 1-3, 5-9, 11-13, 15-16, 18, 20-22.

At least 20% of the total residential housing units (up to 190 dwelling units, if the total units within the project is 945) shall be designated as Affordable Housing, with a percentage of homes deed-restricted to several income categories, including “moderate”, “low” and “very-low” income categories. Additional information is provided in the Development Agreement. Affordable housing is intended to be located on blocks 4, 17, and 19.

**Seniors Housing**

A 150-unit Continuing Care Retirement Complex (CCRC) with 225 beds is included in the project. The CCRC would provide independent living and assisted care for seniors, with common dining, recreational activities, housekeeping and transportation services. The Senior Housing development is planned for Block 10.

**Ground Floor Retail (Residential Above)**

The area fronting the existing dry docks will include 40,000 square feet of neighborhood-serving retail and restaurant uses in mixed-use buildings. Up to 15,000 square feet for restaurant space and up to 25,000 square feet of retail space shall be permitted. The permissible uses would include those that provide goods and services to local residents. These uses would be located on portions of Blocks 11-13, and Blocks 15-16. Residential units may be constructed above these retail spaces.

**Hotel and Accessory Uses**

A 150-room hotel with accessory uses will be located in the southern portion of the 63-acre site west of the railway. Accessory uses include a meeting space, a spa, and dining facilities. The hotel will be located on Block E.

**Office (west side)**

Up to approximately 10,000 square feet of office space will be located in the same general area as the hotel, Blocks C and D.

**Parks and Open Space (west side)**

The project features multiple types of parks and open spaces. On the west side of the railroad tracks 20.7 acres shall be devoted to these uses. Schematic designs and additional details can be found in Section VIII of this Development Plan.

*Areas East of the Railroad Tracks*

**Office/Research & Development/Warehouse and/or Light Industrial**

Approximately 75,000 square feet of industrial, R & D, and/or warehousing is permitted on the eastern parcels. Additional office development of up to 90,000 square feet may be allowed in these same areas on Blocks A and G.

**Membership Warehouse Club Retail**

A membership-based wholesale retail warehouse use (such as Costco) will be located on Block F. The use may be up to 154,000 square feet in size with associated parking and a gas station.

**Study Area**

A 19-acre area has been set aside for future community needs. The Study Area is indicated as Block B on the land use plan and may be developed with industrial uses to those allowed in Blocks A and G.

**Parks & Open Space (east side)**

On the east side of the railroad tracks, up to 13.7 acres shall be committed to these uses, which includes a community farm of approximately 3.5 acres. Schematic designs and additional details can be found in Section VIII of this Development Plan.



III.2 DENSITY AND INTENSITY OF USES

Figure III.2.a - SAMPLE RESIDENTIAL TEST FIT



Residential	
Block #	Assumed Unit Count
1	+/- 42
2	+/- 48
3	+/- 51
4	+/- 42
5	+/- 48
6	+/- 42
7	+/- 51
8	+/- 48
9	+/- 42
10	[150 CCRC] *
11	+/- 46
12	+/- 51
13	+/- 42
14	0
15	+/- 46
16	+/- 36
17	+/- 66
18	+/- 57
19	+/- 54
20	+/- 70
21	+/- 18
22	+/- 45
Total: 945 du	

\* Note: CCRC is not included in total unit count

Non Residential Uses	
A, G	75,000 sf of R&D, Warehouse and Light Industrial 90,000 sf of Office
C, D	10,000 sf of Office
E	150-suite Hotel and associated uses
F	154,000 sf membership warehouse store
11, 13, 15, 16	40,000 sf of Neighborhood Retail

Block Development Capacities

Blocks in the Residential Mixed-Use District (i.e. blocks 1-22) have assumed development capacities summarized in the table at left. Some variation in unit counts per block is expected, but total counts will not exceed the total cap of 945 units and will not exceed the limits imposed by the Development Agreement for any given phase. Block 10 is designated as a location for a 150 unit senior housing development.

Sample Residential Test Fit

The site plan in Figure III.2.a shows a unit count test fit exercise for the NP-MUR-W:AC district of the Napa Pipe Neighborhood. This exercise shows the site as all Townhouses with Mews units, except for the Senior Housing/CCRC and the units above retail along the drydocks. Refer to Design Guidelines and Form-Based Code for typologies.



III.3 MINIMUM NEW GRADES

A grading plan has been established and presented in the referenced sheets. Street elevations have been developed to meet required engineering standards and pad elevations have been developed to provide finished floor elevations above the floodplain.

The current flood elevations are between 7.9’ and 8.4’ above sea level at the site. The study provided by Philip Williams & Associates (PWA) suggested a future flood elevation of 12.0’ based on the sea level rise analysis results for approximately 50 years out. The plan reflects a base site elevation after fill established at 12’ above mean sea level.

In order to protect development and allow for flood insurance coverage even within this 50 year potential sea rise the building pads will be a minimum of 1’ above this 12’ flood elevation.

Typical elevations above the base finish grade are therefore assumed to be:

- 0.5 ft above flowline of road = sidewalk
- 0.5 to 1.0 ft above sidewalk = building pad

Therefore the minimum elevation for a slab on grade floor plate is 13’ to 13.5’ above mean sea level.

Additionally, many of the residential typologies are developed to include stoops and porches which may raise the finish floor elevation an additional 2’ to 2.5’ plus (prototype design shows a finish floor at 15.5’ elevation), in order to provide active street facades.

It is important to note that certain building entries and units may be built at the 13’ elevation to facilitate at-grade accessible entries. This is particularly true of the neighborhood serving retail with its direct accessible entries.

After grading is completed, emergency respose will be provided throughout the development.

Refer to Tentative Map: Grading Plans - Sheets 11, 12, 13, 14, 15, and 16

Grading Narrative

The grading concept is based on deploying a set of grading strategies that are unique to the various site elements that collectively form the site, these elements include:

- Block Parcels
- Roads and Sidewalks
- Napa River Edge
- Existing Wetlands & Created Wetlands
- Parks
- Railroad ROW

Each grading strategy is designed to address a series of overarching goals that include minimizing stormwater run-off, maximizing infiltration of stormwater, minimizing the quantity of stormwater that is released to the County stormwater system, and minimizing disturbance to existing wetland and shoreline vegetation. Therefore, where possible impermeable paving is pitched to permeable softscape and the overall quantity of hardscape is minimized. The following is a summary of how these goals are achieved for each of the site elements listed above.

General Fill Program

The majority of the site will be raised by approximately 5’; this includes all block parcels, streets, and most parks. The exceptions to this include the Railroad ROW, the shoreline, the community park (P6), and the existing wetlands, which will all maintain existing elevations. Upon completion of grading, emergency response will be provided throughout the development.

Block Parcels

A flat pad will accommodate and facilitate the construction of the various building types. All stormwater that falls within the block parcels will be processed by either interior rain gardens, permeable pavements, flow-through planters or other vegetated areas and then piped to domestic stormwater systems.

Roads and Sidewalks

The grading for all streets and sidewalks directs stormwater to bio-filtration areas in the streets. There are two grading conditions including 1) cross pitched streets and cross pitched sidewalks sloped to continuous bio-filtration planters running along the length of the street or 2) crowned streets and cross pitched sidewalks sloped to bio-filtration planters in the center of the blocks, whereby the intersections are highpoints. This approach prevents water from pooling at the pedestrian crossings at street intersections. The grading of the bio-filtration areas is illustrated in the tentative map details.

Napa River Edge

For the area to the south of the drydocks, all existing slopes along the river are maintained and the new grades feather into the apex of the existing slopes. This approach preserves all existing vegetation along the slopes. For the Community Park to the north of the drydocks (P6), the existing grades will remain and a new retaining wall will be installed on the eastern edge of the park. A minimum 12’ elevation is required for new walls around P6. This will match the adjacent street grade.

Existing Wetlands and Created Wetlands

Existing wetland setbacks have been established including a 10’ no touch setback, a 25’ fill only setback and a 50’ no structures setback. Passive uses, including a pedestrian path between the 25 and 50 foot setbacks are acceptable. All existing grades for the mapped wetlands including all land within a 10’ setback from the edge of the wetland will be preserved and protected. Therefore, the new grades will feather into the wetland slopes beyond the 10’ setback line.

Parks

The parks will be graded to have mounds and swales to direct and funnel run-off to a variety of bio-filtration areas (i.e. rain gardens), thereby minimizing the need for traditional catch basins in the pavements. The shaping of the earth will also provide aesthetic interest and variety for the overall site which will appear predominantly flat.

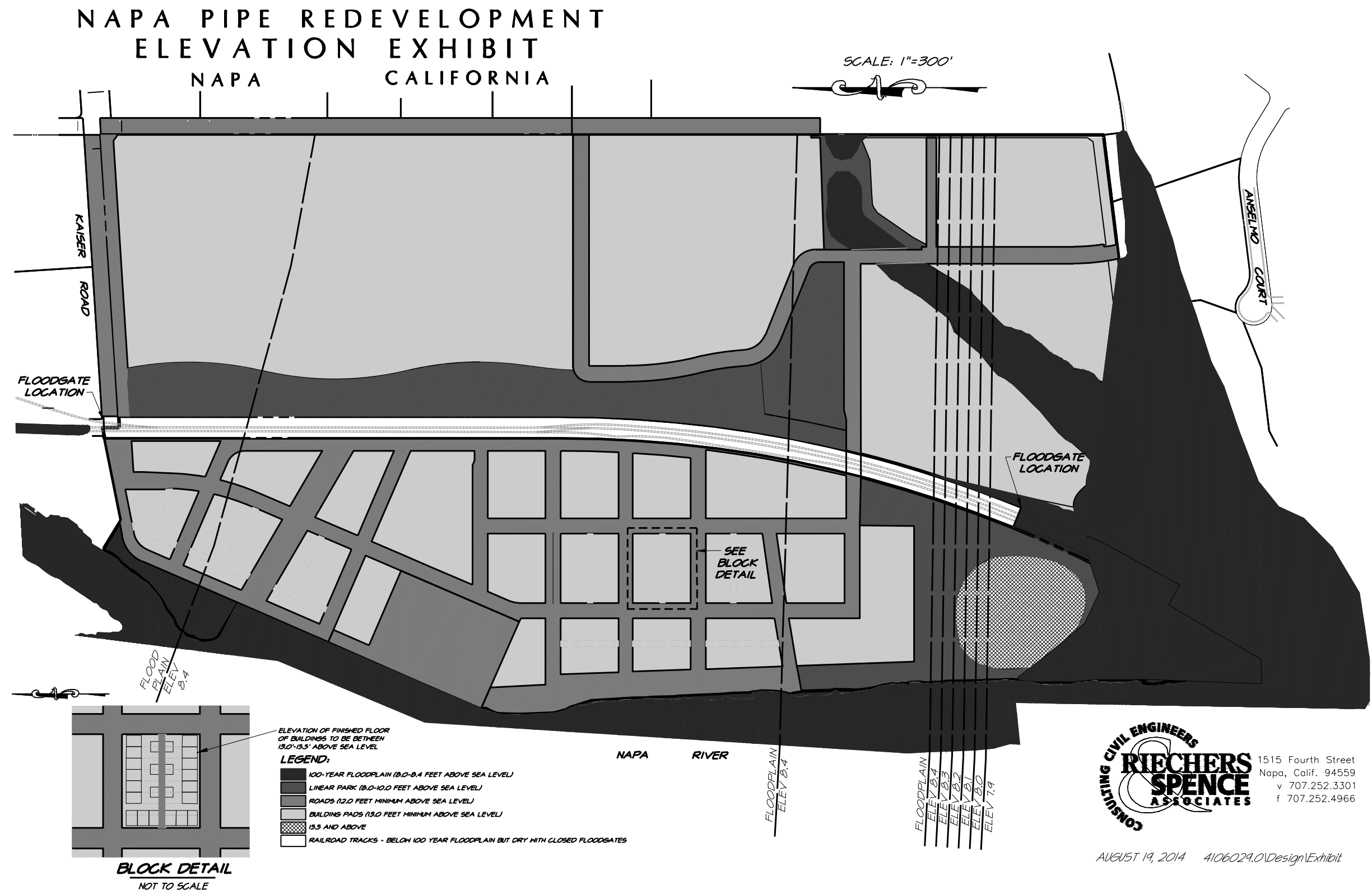
Railroad ROW

The existing grades at the railroad ROW will be preserved. Because the majority of the site is being raised by 5’, the streets and sidewalks providing access to the railroad crossings will be graded to provide a maximum of 5% slopes down to the crossings.

High Water Event

In its current state the northernmost railroad crossing is above the 100-year floodplain and will remain navigable during a storm of that magnitude. The southern crossing will be inundated during a 100-year storm event. The use of floodgates is proposed to allow two points of ingress, egress during a large storm event. The northern gate is proposed for the sea level rise scenario; the southern gate is needed now and in the future. Both gates would be designed to accomodate the sea-level rise scenario.

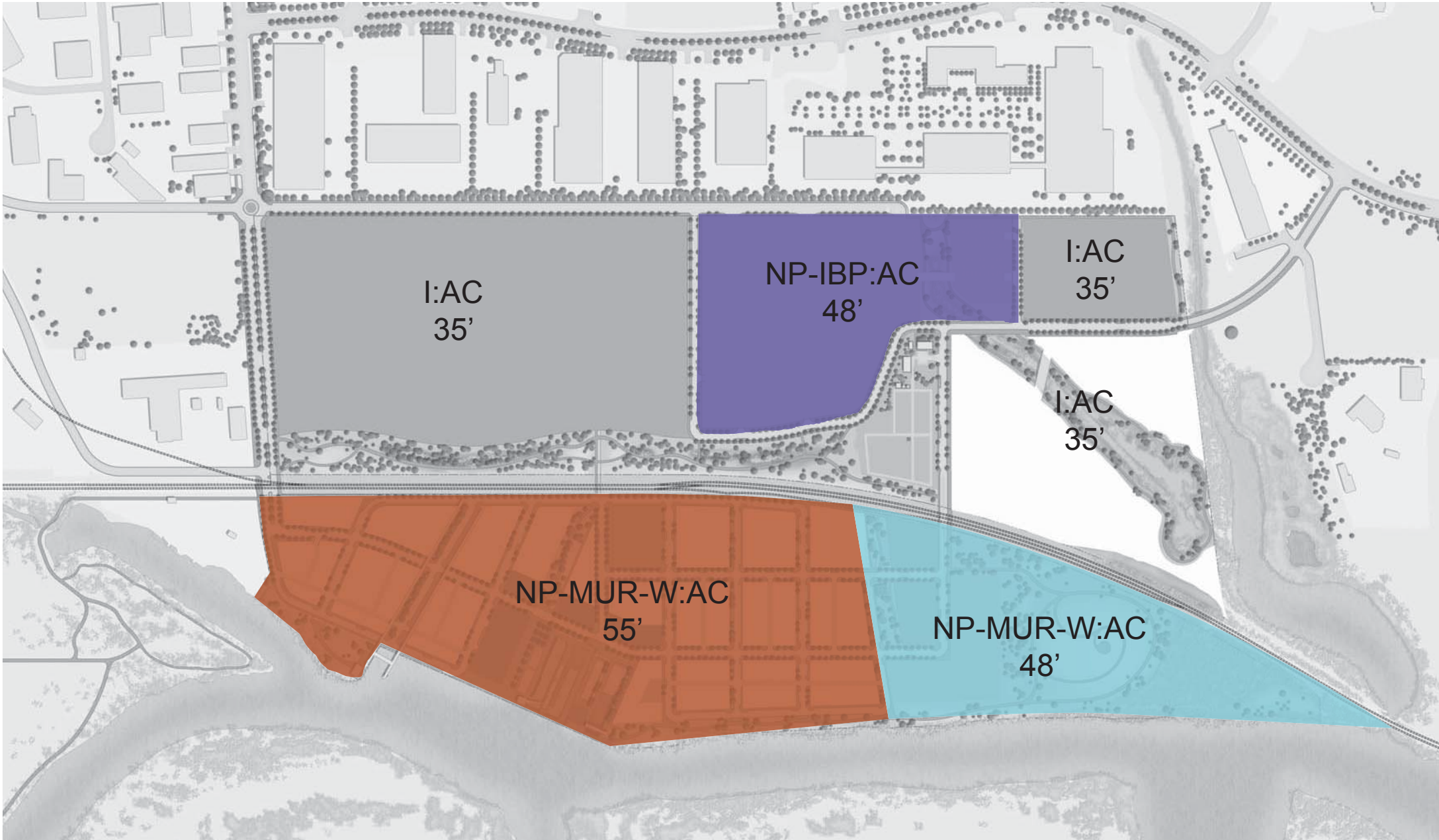
Figure III.3.a - SITE ELEVATIONS EXHIBIT





III.4 MAXIMUM HEIGHT PERMITTED BY ZONING ORDINANCE

Figure III.4.a - MAXIMUM HEIGHT PLAN



The maximum height in the NP-MUR-W:AC district shall be 55 feet above base finish grade.

The maximum height in the NP-IBP-W:AC district shall be 48 feet above base finish grade.

The maximum height in the NP-IBP:AC district shall be 48 feet above base finish grade.

The maximum height in the I:AC district shall be 35 feet above base finish grade.

- KEY:
- NP-MUR-W District: See Napa County Zoning Ordinance Ch. 18.66.060. Height Limited to 55'
  - NP-IBP-W District: See Napa County Zoning Ordinance Ch. 18.66.140. Height Limited to 48'
  - NP-IBP District: See Napa County Zoning Ordinance Ch.18.66.230. Height Limited to 48'
  - I District: See Napa County Zoning Ordinance Ch.18.104.010. Height Limited to 35'



III.5 STORMWATER MANAGEMENT REQUIREMENTS

Figure III.5.a - PROPOSED LOCATIONS FOR BEST MANAGEMENT PRACTICES



A schematic depiction of the stormwater management measures has been developed and demonstrates proposed measures to accept, treat, store and discharge stormwater by accepted engineering standards. This includes the use of:

- bioretention,
- flow-through planters,
- vegetated swales,
- cisterns,
- green roofs, and
- pervious pavements.

The Best Management Practices (BMPs) for the proposed development are provided with demonstrations of intended use, component design, installation and upkeep per the appropriate governing agencies and accepted industry standards.

Refer to Figure III.5.a - Proposed Locations for Best Management Practices and Vol. 2 Exhibit D - Stormwater Runoff Management Program (SRMP), prepared by Riechers Spence & Associates, dated November 8, 2013.

LEGEND

- ..... Wetland Buffer
- Constructed wetlands
- Existing wetlands
- Commercial, Retail, and Light Industrial Areas Treated by a Combination of Bioretention, Vegetated Swales, Flow-through Planters and Green Roofs
- Bioretention Area/ Vegetated Swale/ Green Space
- Street Treated by Bioretention
- Alleyways Treated by Pervious Pavement



IV.1 - PUBLICLY-ACCESSIBLE PARKS AND OPEN SPACES

Figure IV.1.a - SITE PLAN SHOWING COMMUNITY AMENITIES AND HISTORIC FACILITIES





IV.1 - PUBLICLY-ACCESSIBLE PARKS AND OPEN SPACES

Figure IV.1.b - SITE PLAN SHOWING PUBLICLY-ACCESSIBLE PARKS

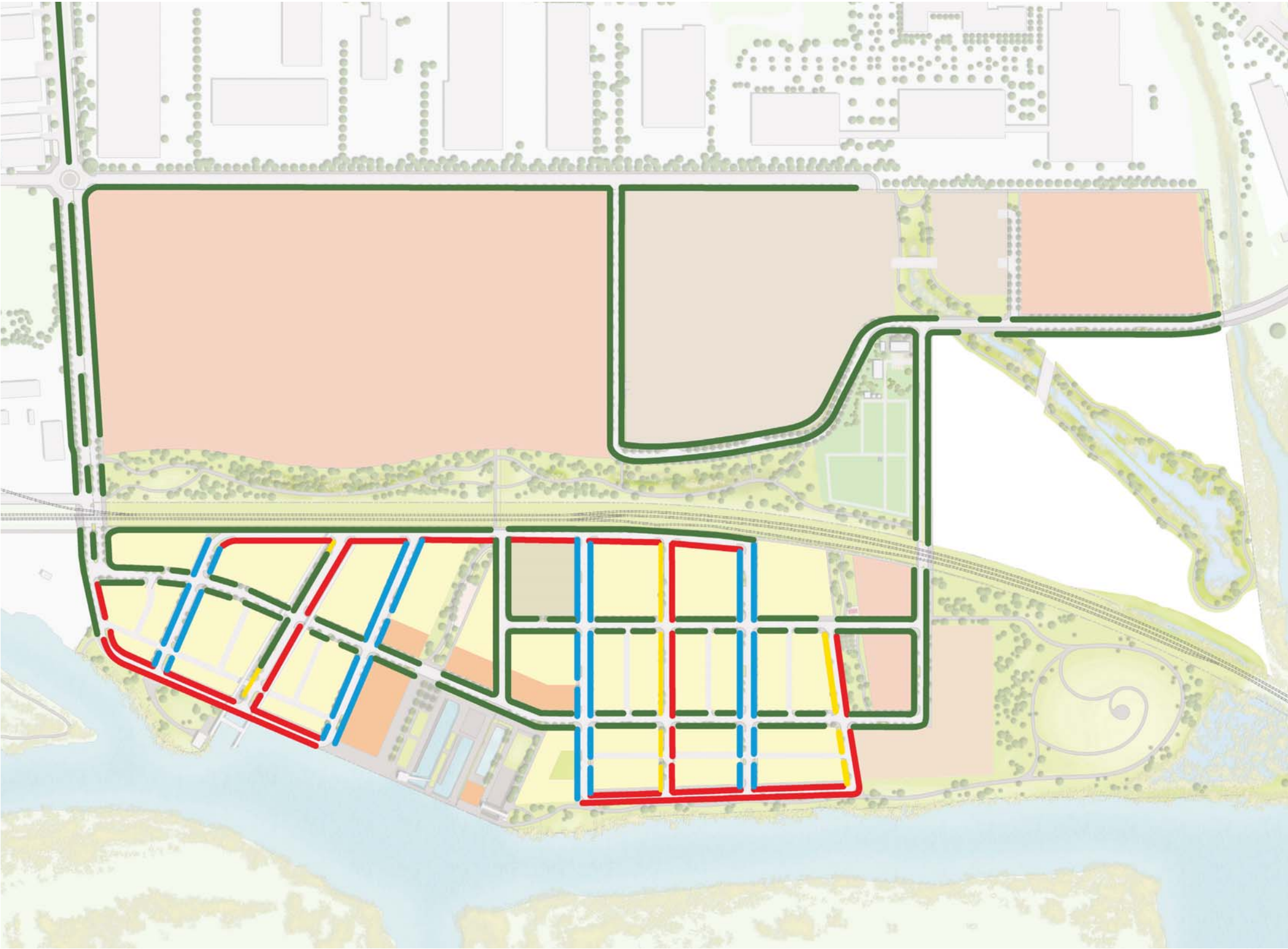


- LEGEND**
- P1 - KNOLL, ADJACENT WETLANDS
  - P2 - RIVERFRONT PARK, TRAIL
  - P3 - FARM
  - P4 - LINEAR R.R. PARK (SOUTHERN)
  - P5 - LINEAR R.R. PARK (NORTHERN)
  - P6 - COMMUNITY PARK
  - P7 - DIAGONAL PARK TO DRYDOCK
  - P8 - SOUTH GREEN
  - P9 - RESTORED WETLAND PARK,



IV.1 - PUBLICLY ACCESSIBLE PARKS AND OPEN SPACES

Figure IV.1.c - STREET TREE PROGRAM



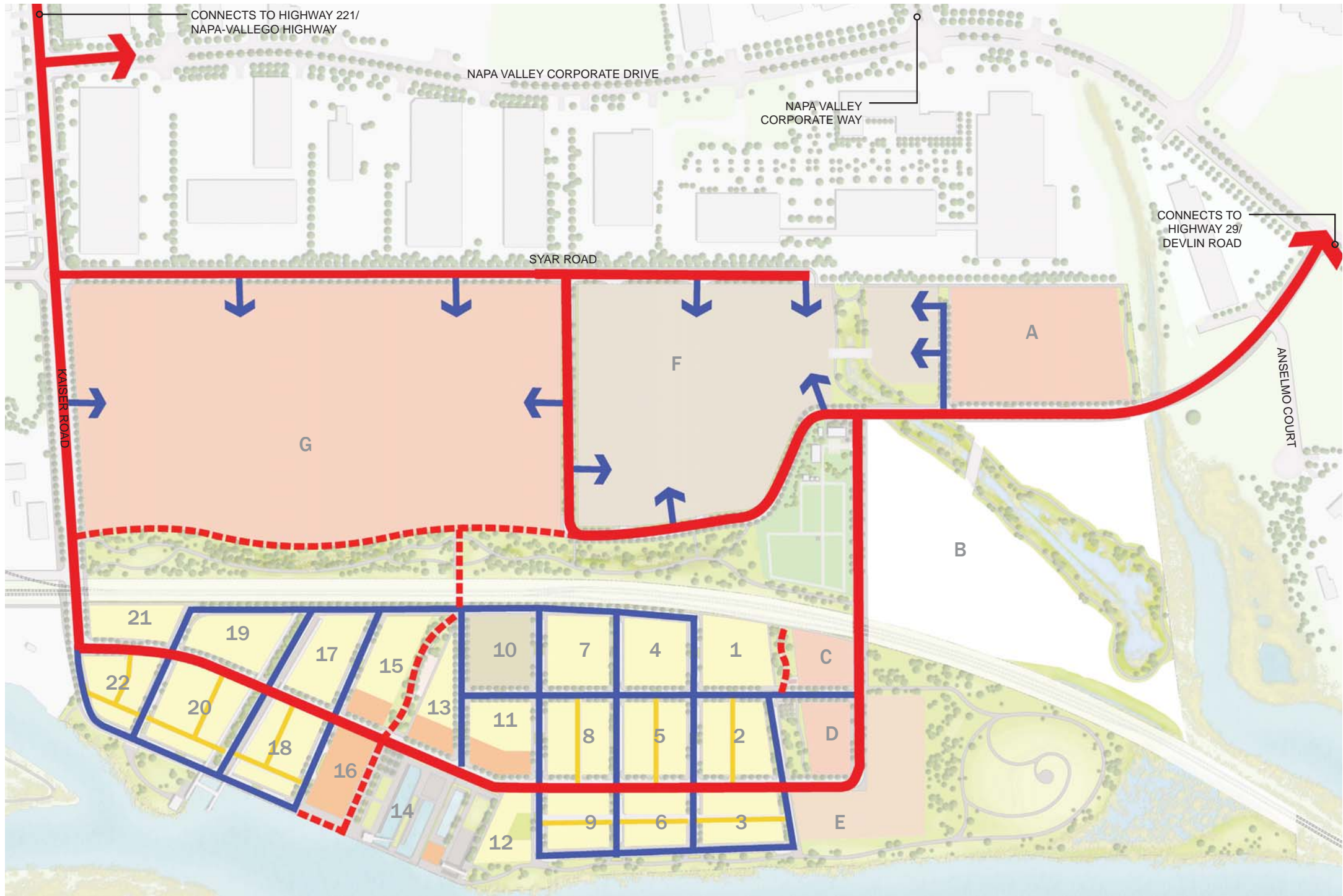
This diagram identifies the variety in street tree massings. For example the biofiltration planters will have clustered street trees, while there would be no street trees allowed on the east side of the blocks facing the railroad park or along the waterfront in order to maximize view out from the residences.

- LEGEND**
- Regularly spaced street trees
  - Irregularly spaced street trees
  - Clustered street trees
  - No street trees



V.1 SITE ACCESS & CIRC
 LATI

Figure V.1.a - CIRC
 LATI
 HIERARCH



Studies have been conducted showing acceptable clearances for the maneuvering of large service vehicles including:

- Pierce Model-Stock 105 Heavy Duty Ladder Trucks,
- Labrie Automizer Garbage Trucks and
- Lifeline F450 167" Ambulances.

Diagrams showing main site access, circulation within site, and pedestrian/bicycle trail circulation and connection are provided.

The street and pathway layouts throughout the site are designed to ensure that all buildings and facilities are accessible to the Fire Dept and other emergency vehicles. EVA access routes are provided by streets or vehicular rated sidewalks.

As identified on the enlargement exhibits within the Development Plan, the linear park between blocks 1,C, and D (P8 South Green) incorporates a 20' wide pathway that provides EVA access. The same strategy is used for the linear park between blocks 13 and 15 (P7 Diagonal Park).

The Dry Dock plaza (block 14) is EVA accessible by a road on the east side and vehicular rated sidewalk paving on the north and southern sides. The restaurant would be serviced by a wharf hydrant.

Refer to Figures V.1.b - Site Access & Circulation Plan; V.1.c Bike Trail Circulation Plan; VI.3.p - P8: South Green; VI.3.n - P7: Diagonal Park to Drydock; VI.3.t Block 14: Dry Dock Plaza; and Section VIII Tentative Map: Sheets 4, 5, 6 and 7, attached.

LEGEND

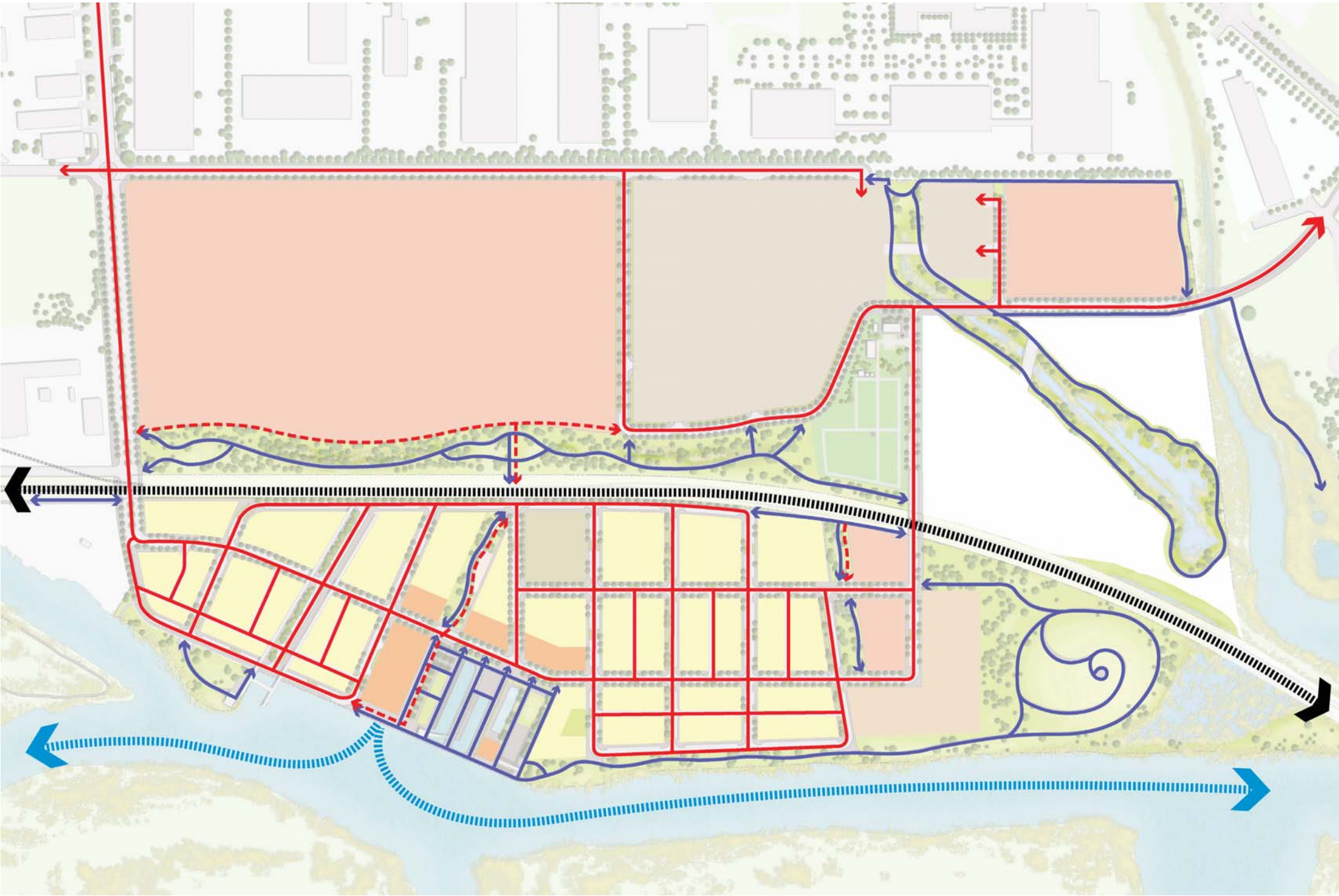
- Primary access
- Secondary access
- Primary streets
- Secondary streets
- Shared driveways
- (EVA) Emergency Vehicular Access - Authorized Vehicles ONLY 20' clear path





V.1 SITE ACCESS & CIRCULATION

Figure V.1.b - SITE ACCESS & CIRCULATION PLAN

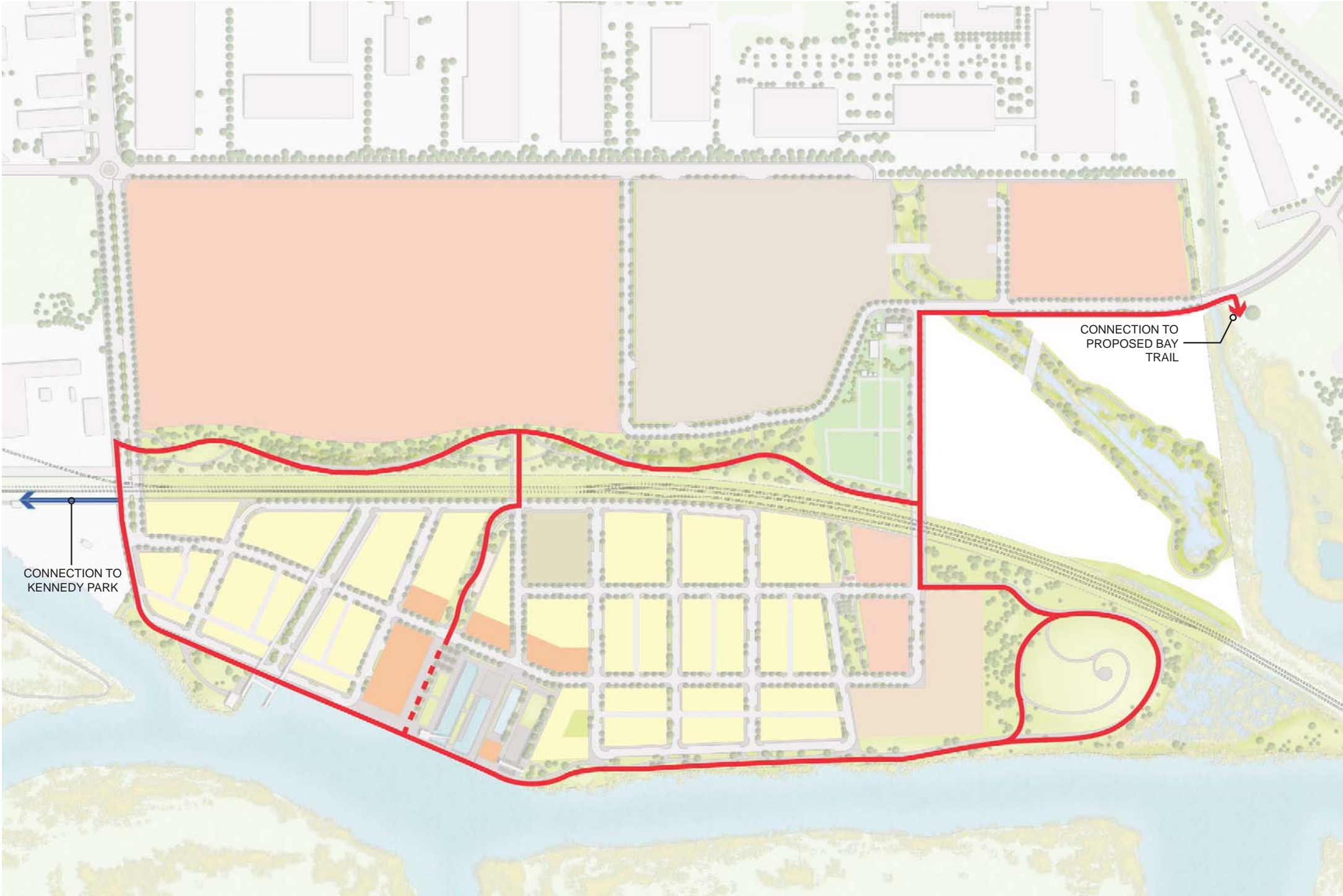


- LEGEND**
- Vehicular & pedestrian
  - Pedestrian & bicycles
  - Railroad corridor
  - Possible water taxi
  - Emergency Vehicular Access (EVA) - Authorized Vehicles ONLY



V.1 SITE ACCESS & CIRCULATION

Figure V.1.c - BIKE/PESESTRIAN TRAIL CIRCULATION PLAN



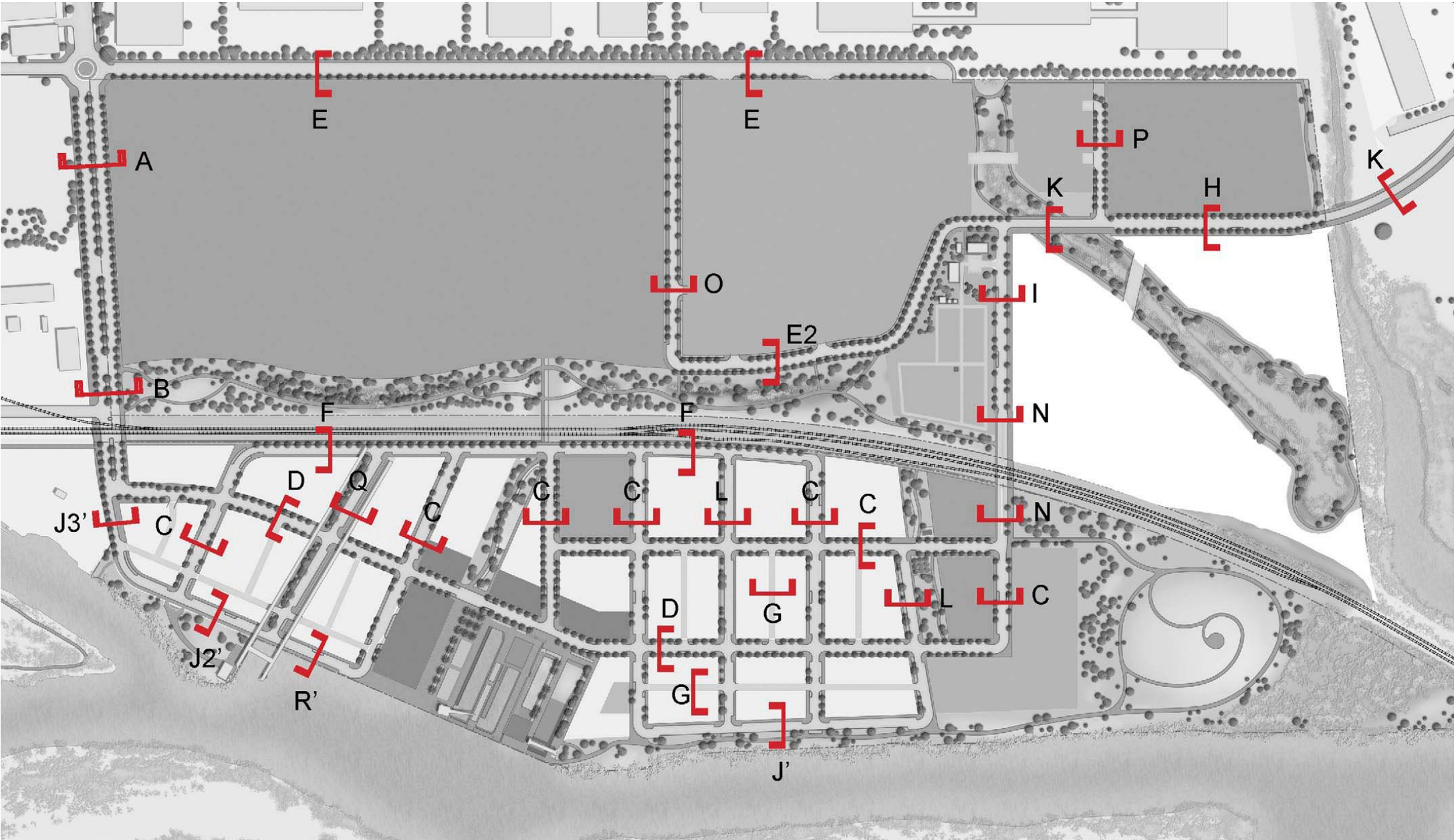
LEGEND

- 12' to 14'-6" shared pedestrian/ bicycle trail
- 10' shared pedestrian/bicycle trail with 2' shoulders on each side
- 12' walk your bike slow zone



V.2 ROAD & STREET STANDARDS

Figure V.2.a - KEY PLAN



Street sections have been developed to:

- make the roads more pedestrian friendly,
- slow vehicle speeds, and
- to allow for stormwater treatment.

The widths of travel lanes and on-street parking generally meet minimum City street standards, sidewalk widths vary, and bioretention areas are included throughout.

**GENERAL NOTE:**

All roads are classified in the following categories:

- Arterial
- Collector
- Local

All local road intersections have 4-way stop signs. Road speed limits are identified under its respective section.

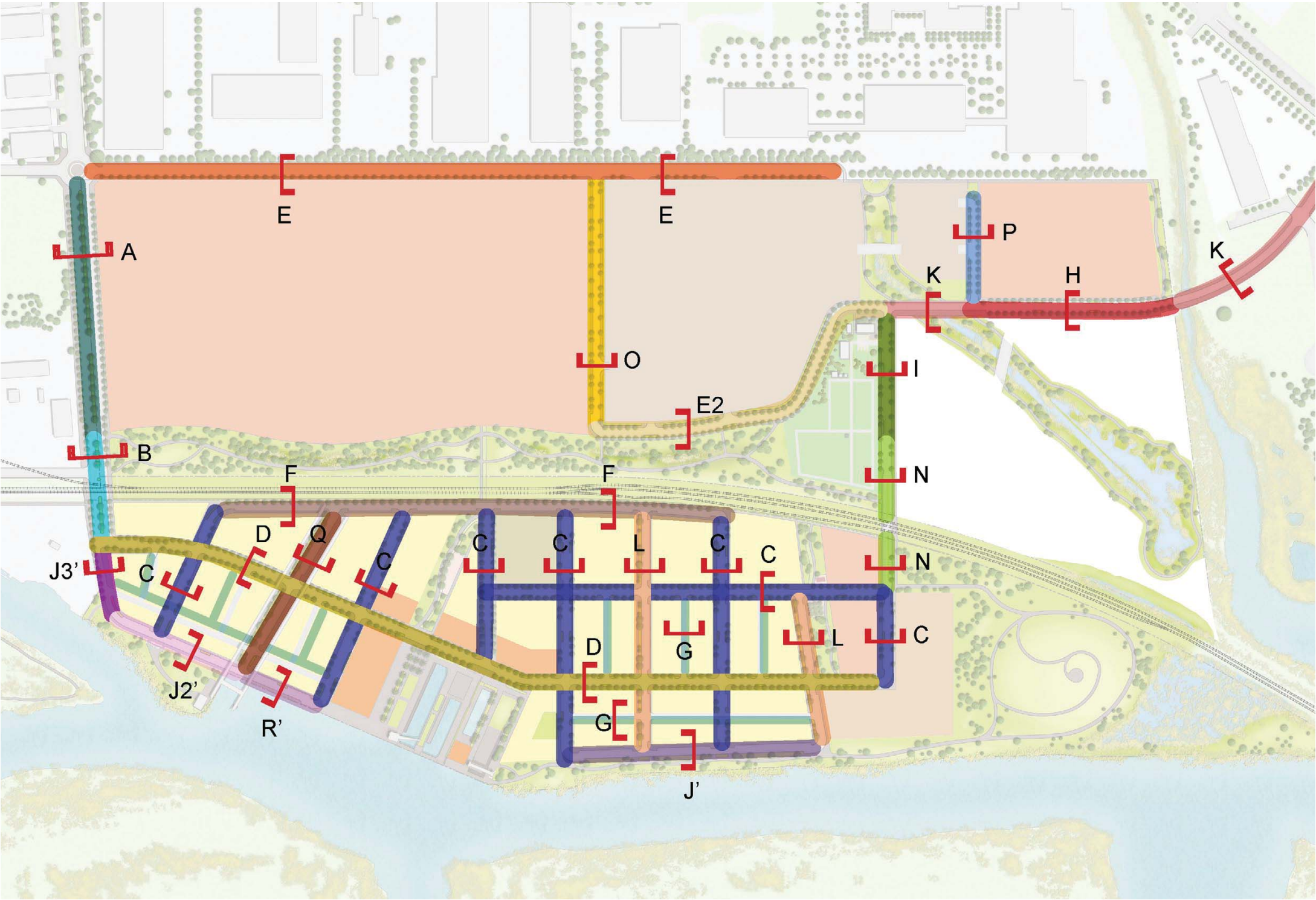
Some blocks include shared driveways.

Refer to Figure V.2 - Road and Street Standards and Tentative Map: Sheet 8.



V.2 ROAD & STREET STANDARDS

Figure V.2.b - SECTION TRANSITIONS



LEGEND

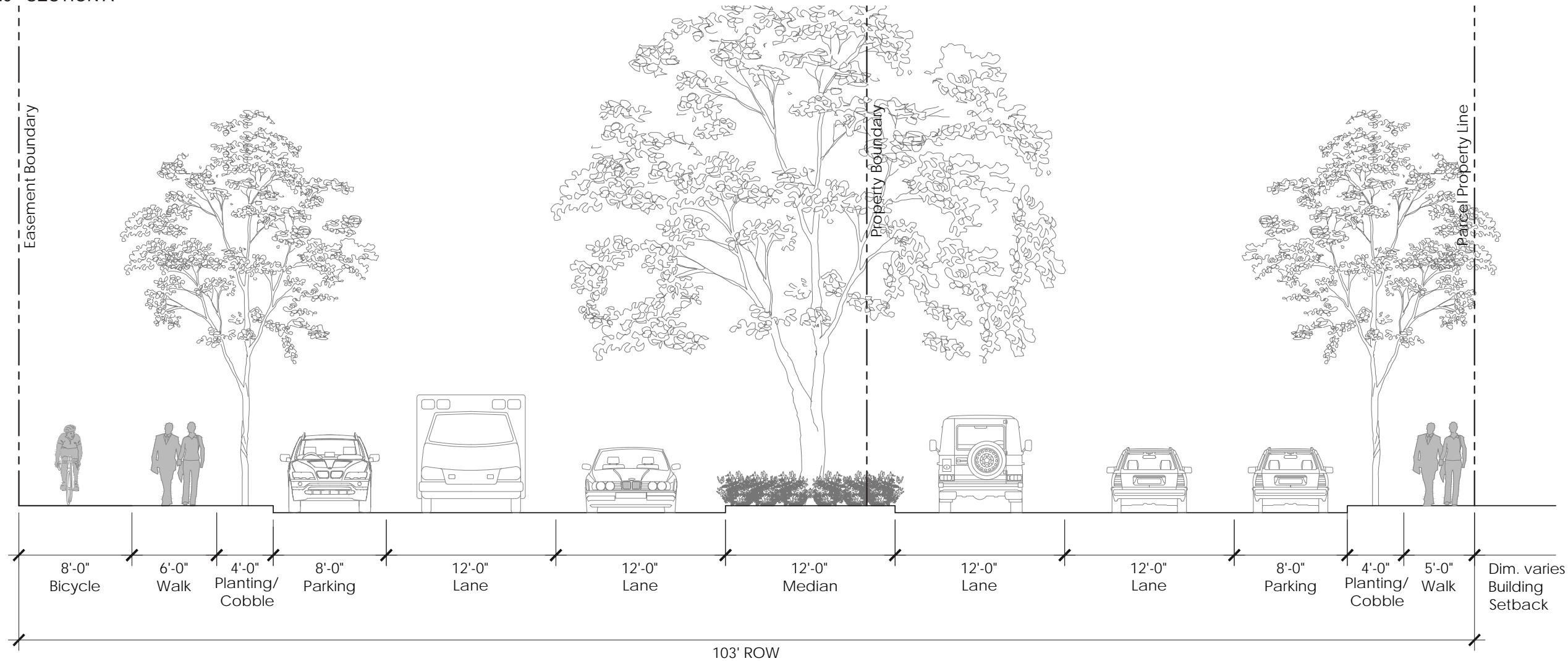
- SECTION A
- SECTION B
- SECTION C
- SECTION D
- SECTION E
- SECTION E2
- SECTION F
- SECTION G
- SECTION H
- SECTION I
- SECTION J'
- SECTION J2'
- SECTION J3'
- SECTION K
- SECTION L
- SECTION N
- SECTION O
- SECTION P
- SECTION Q
- SECTION R

The purpose of this exhibit is to identify the extent of each street section. The change in color identifies the transition from one street condition to the next.

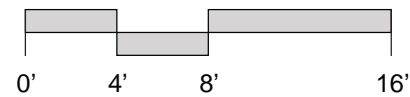


V.2 ROAD & STREET STANDARDS

Figure V.2.c - SECTION A



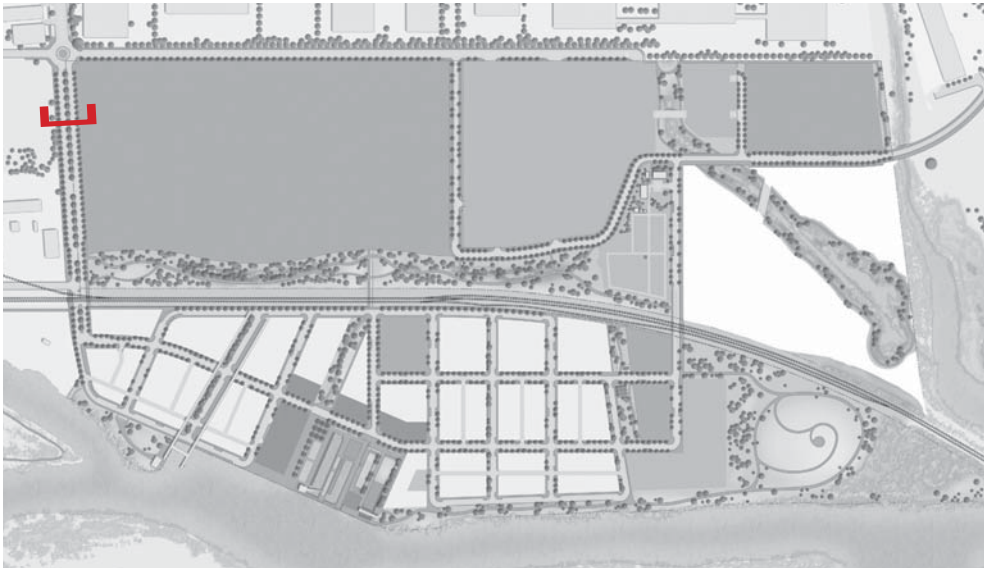
**COLLECTOR ROAD**  
speed limit: 35 mph



**GENERAL NOTE:**

Coordinate street tree locations with all parallel street parking, steps & stoops.

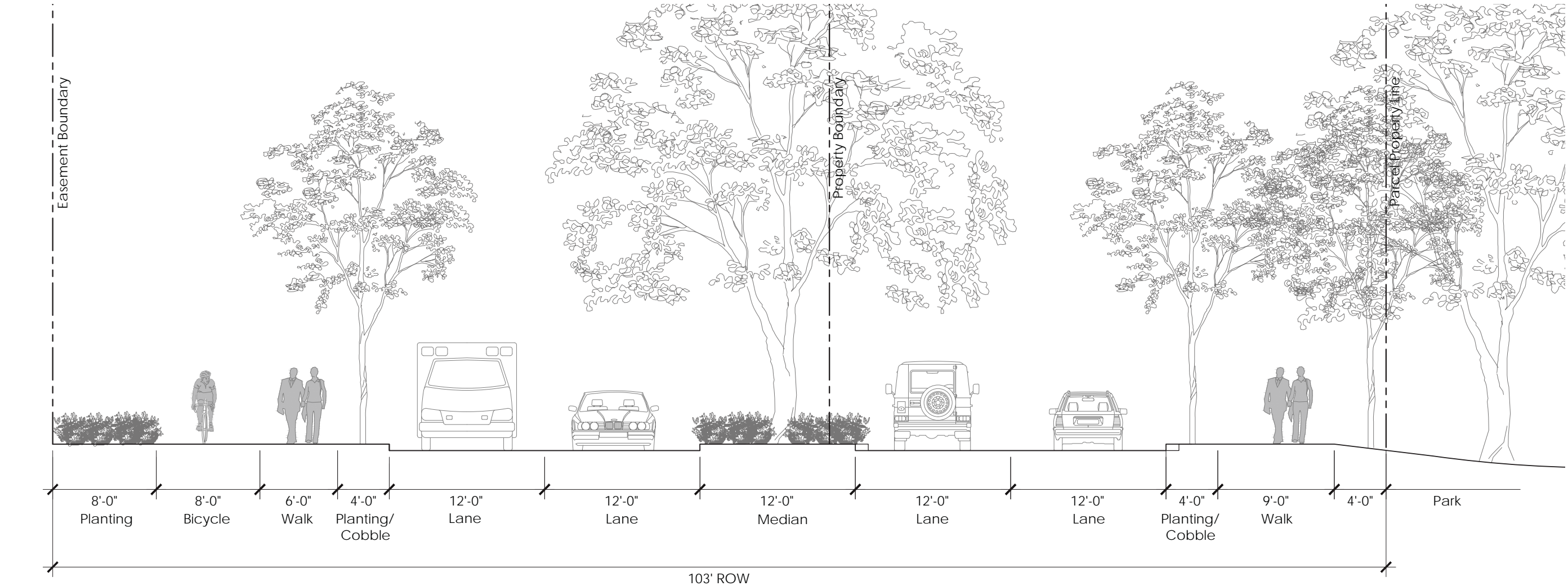
Tree trunk location to be setback a minimum of 24" from face of curb.



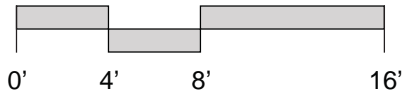


V.2 ROAD & STREET STANDARDS

Figure V.2.d - SECTION B



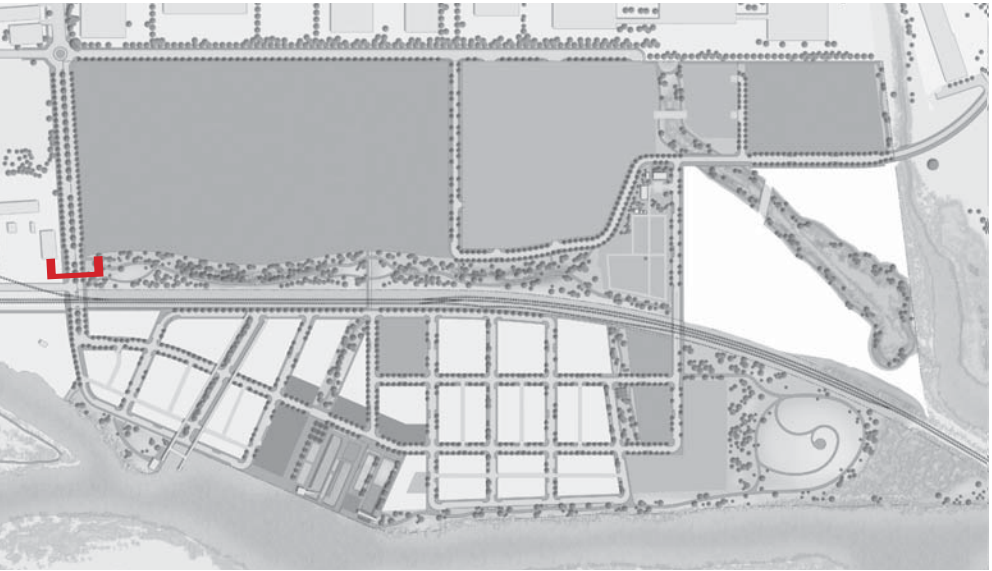
**COLLECTOR ROAD**  
speed limit: 35 mph



**GENERAL NOTE:**

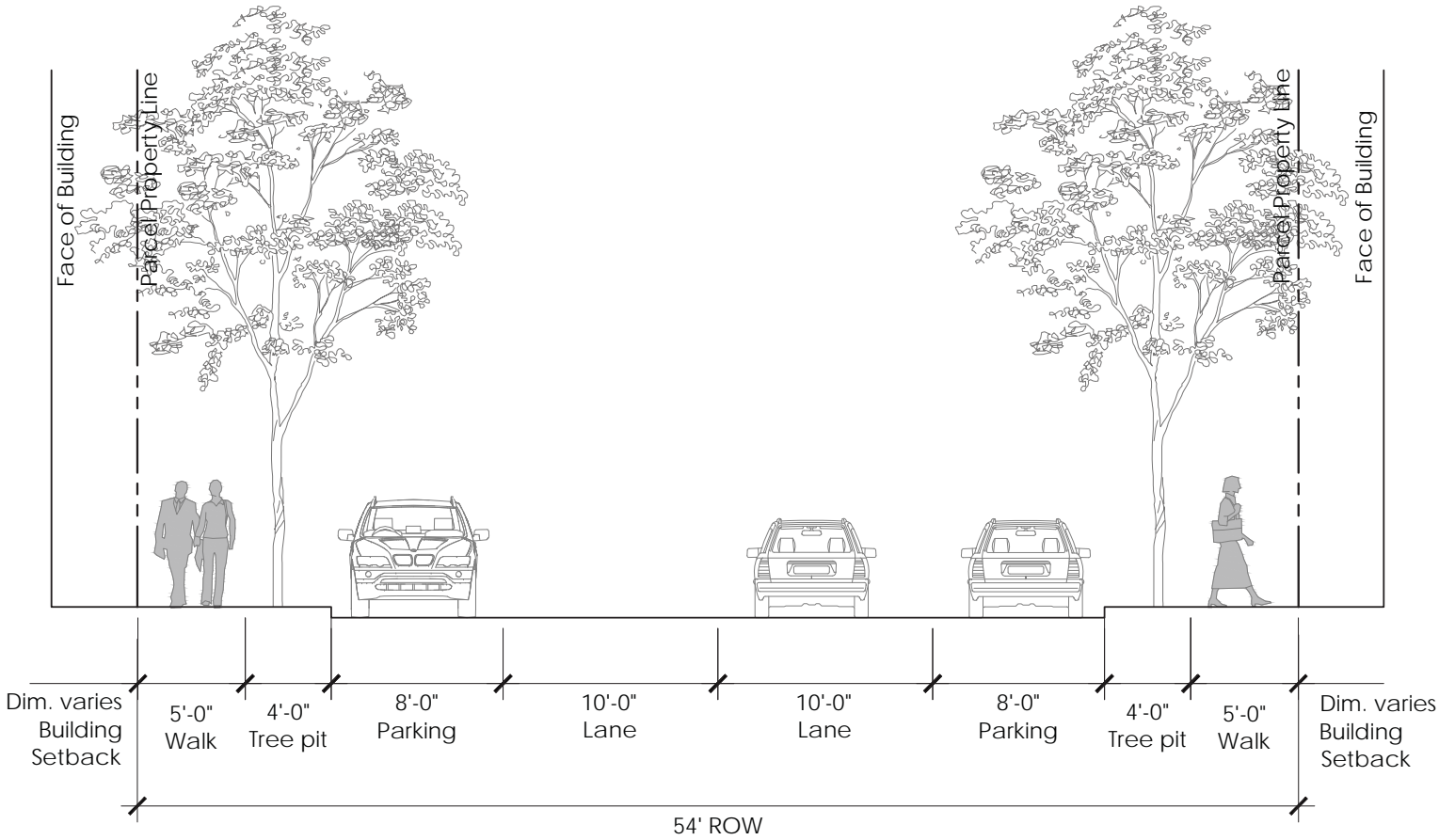
Coordinate street tree locations with all parallel street parking, steps & stoops.

Tree trunk location to be setback a minimum of 24" from face of curb.

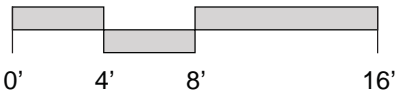


V.2 ROAD & STREET STANDARDS

Figure V.2.e - SECTION C



**LOCAL ROAD**  
speed limit: 25 mph



**GENERAL NOTE:**

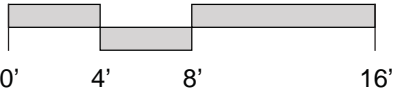
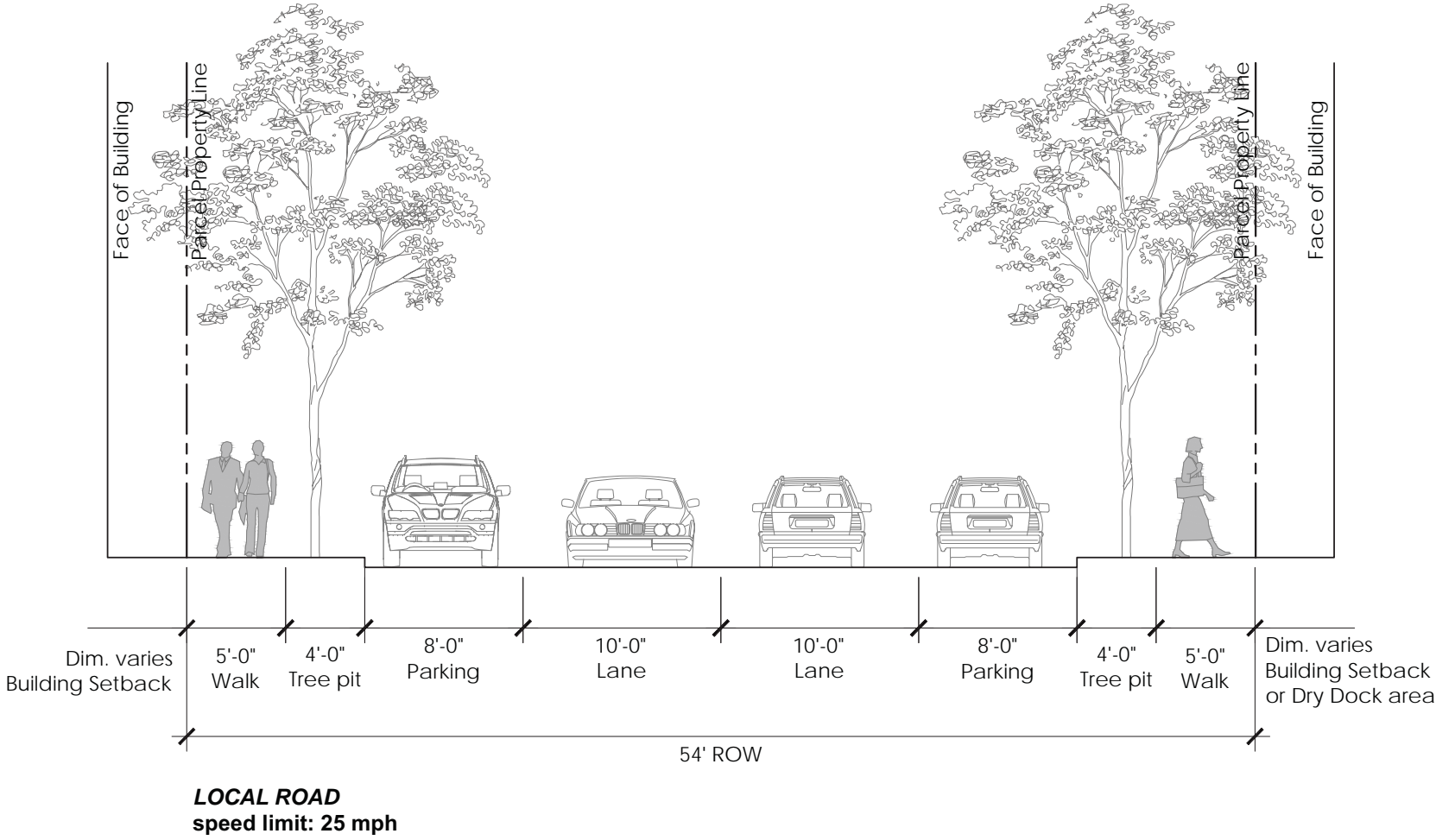
Coordinate street tree locations with all parallel street parking, steps & stoops.

Tree trunk location to be setback a minimum of 24" from face of curb.



V.2 ROAD & STREET STANDARDS

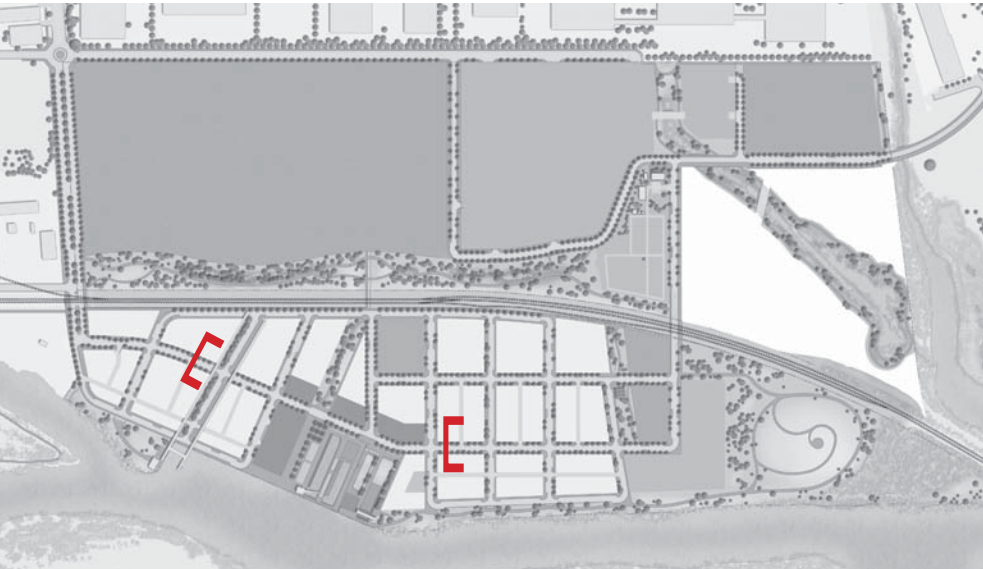
Figure V.2.f - SECTION D



GENERAL NOTE:

Coordinate street tree locations with all parallel street parking, steps & stoops.

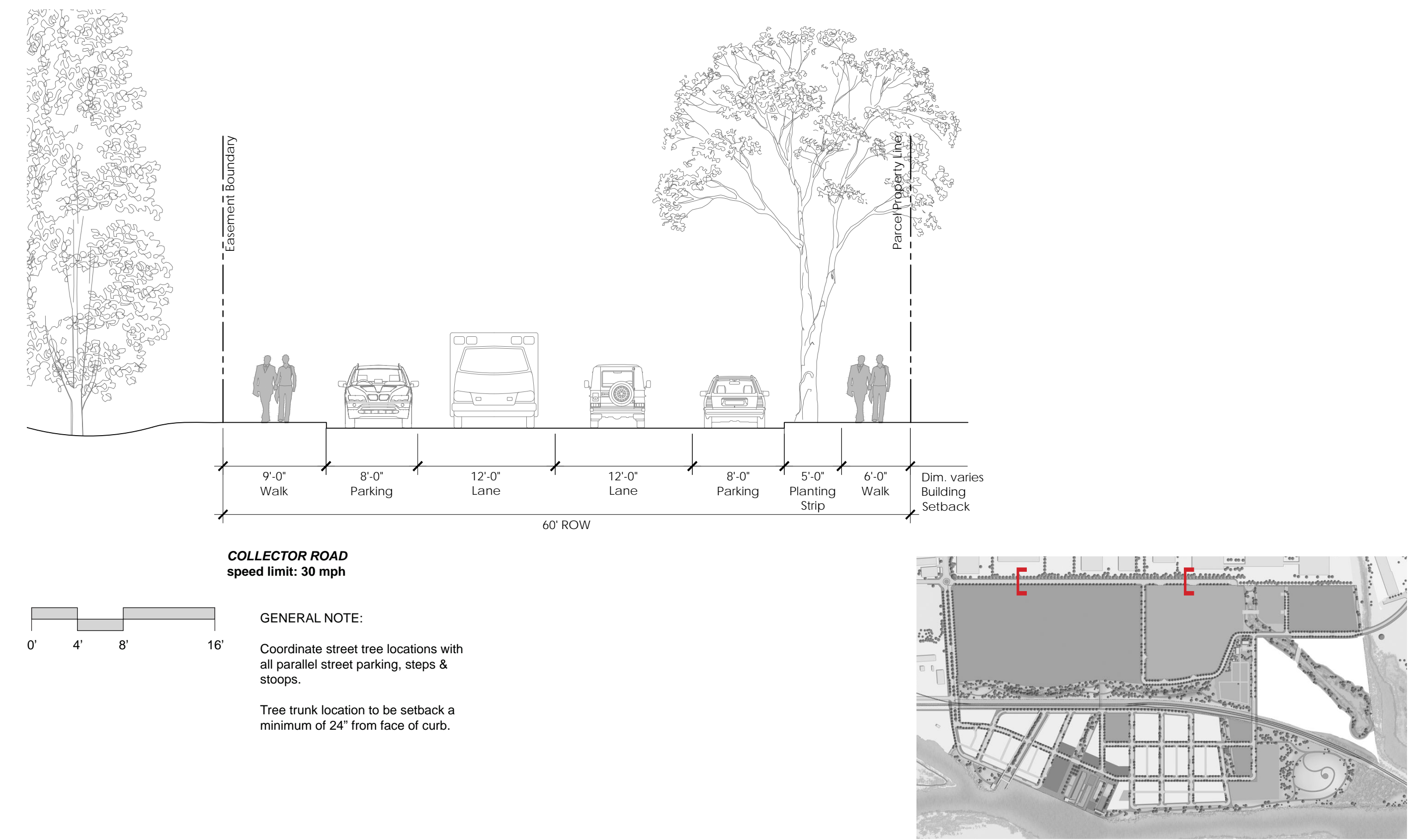
Tree trunk location to be setback a minimum of 24" from face of curb.





V.2 ROAD & STREET STANDARDS

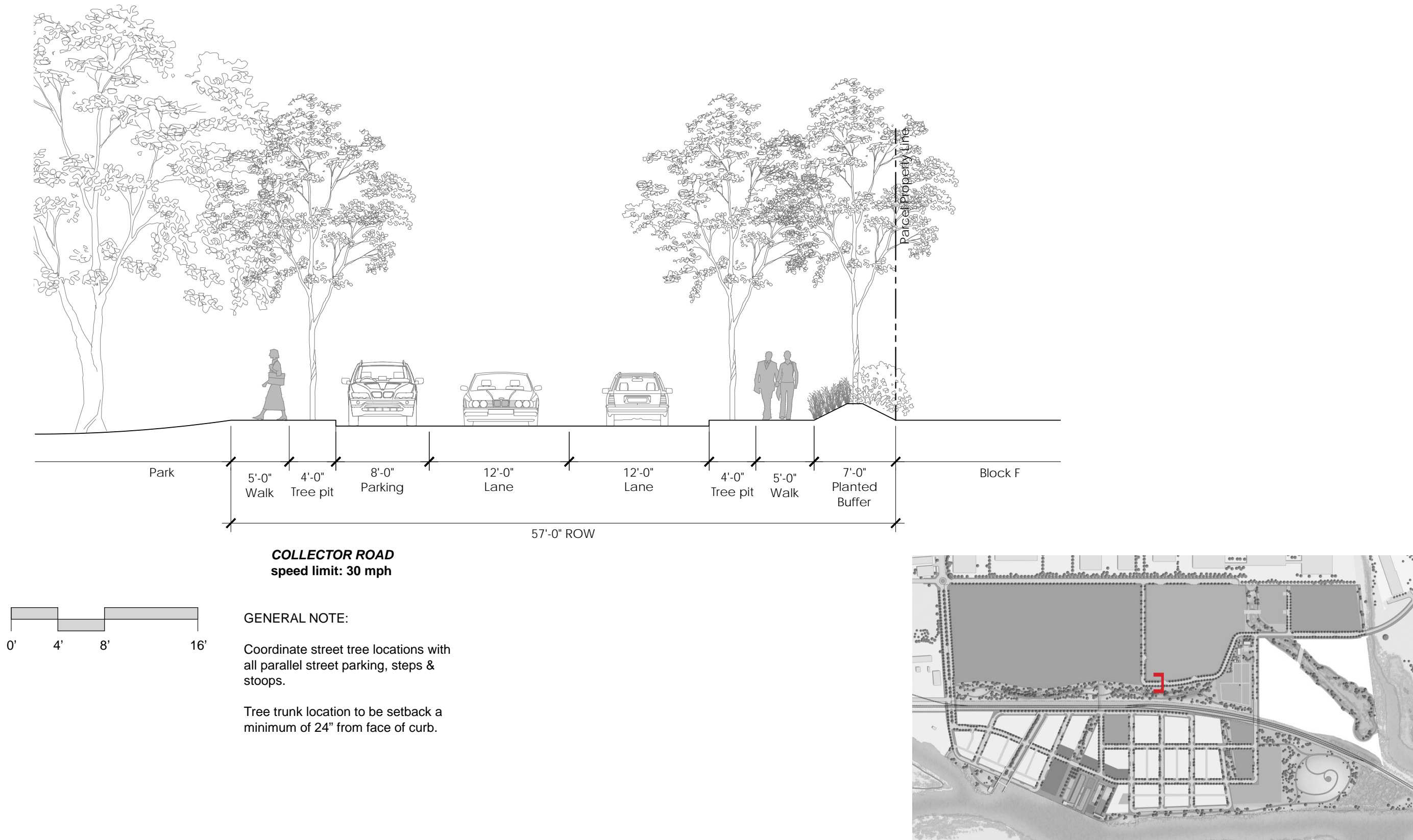
Figure V.2.g - SECTION E





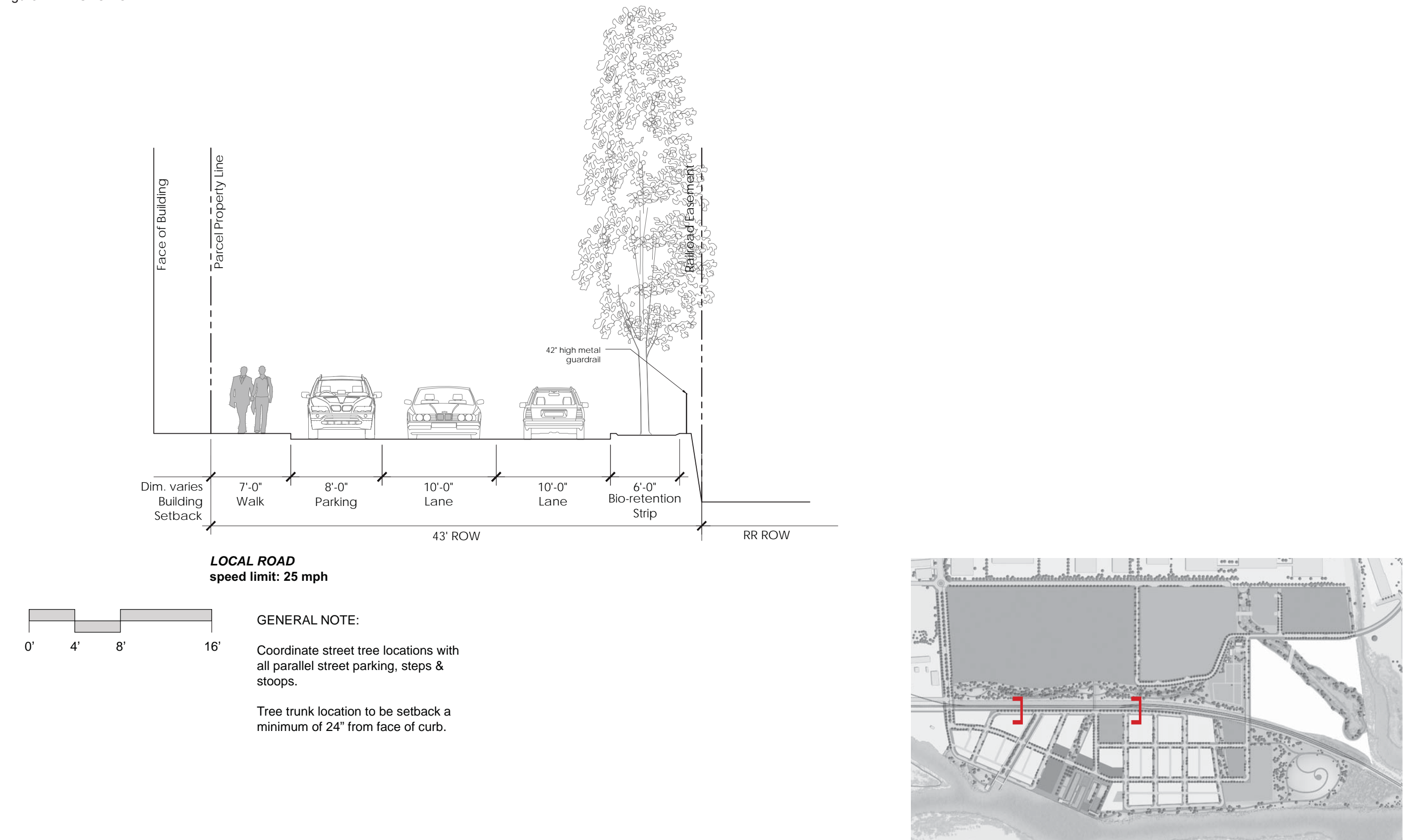
V.2 ROAD & STREET STANDARDS

Figure V.2.h - SECTION E2



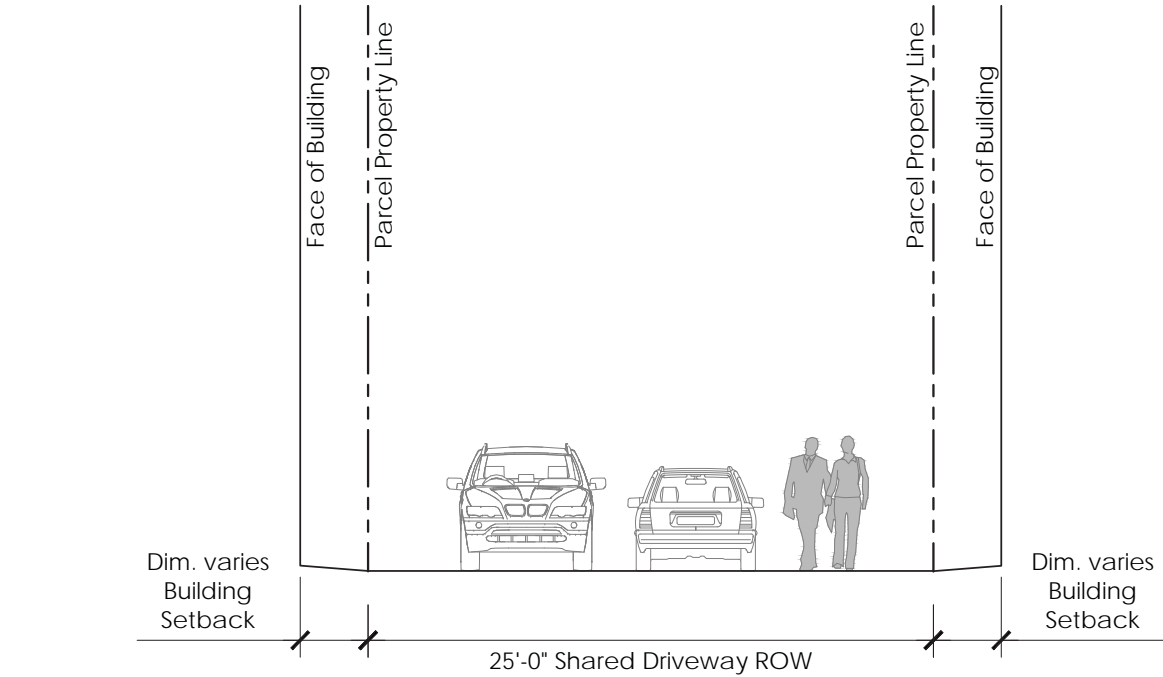
V.2 ROAD & STREET STANDARDS

Figure V.2.i - SECTION F

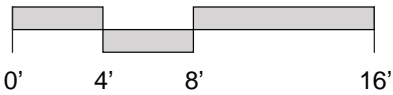


V.2 ROAD & STREET STANDARDS

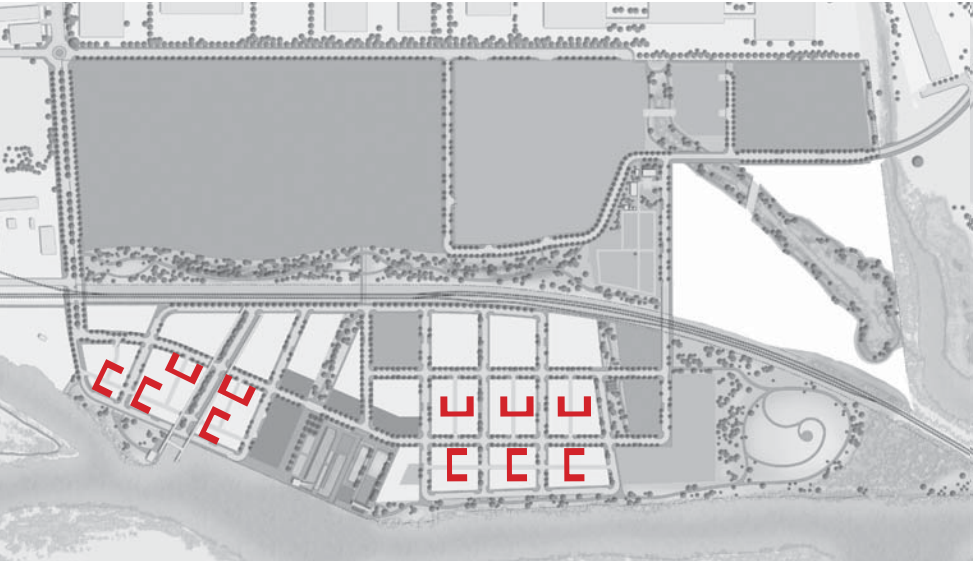
Figure V.2.j - SECTION G



**SHARED DRIVEWAY**  
speed limit: 15 mph



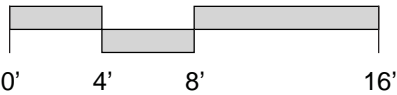
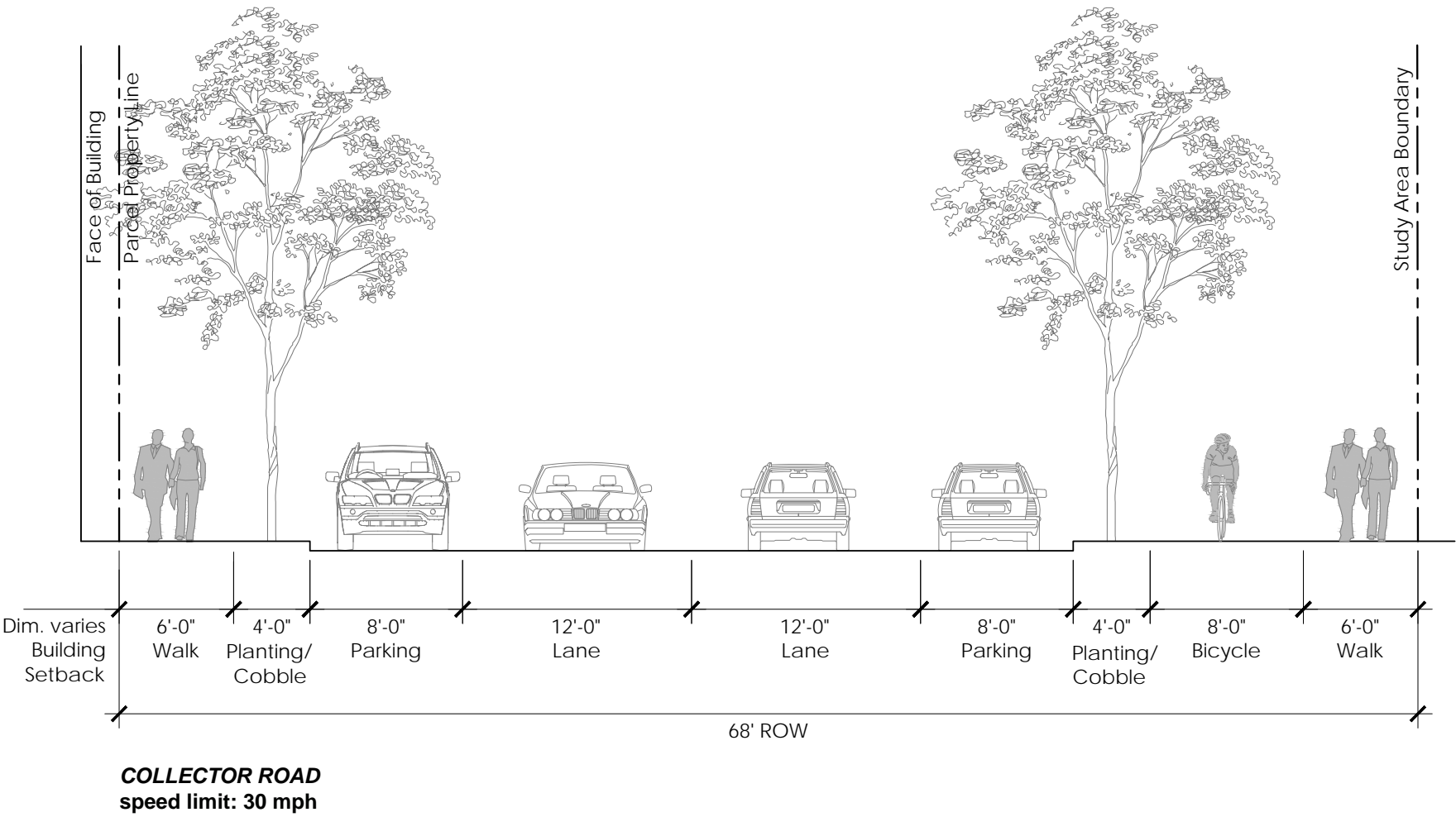
Reference city standards for requirements of driveway design.





V.2 ROAD & STREET STANDARDS

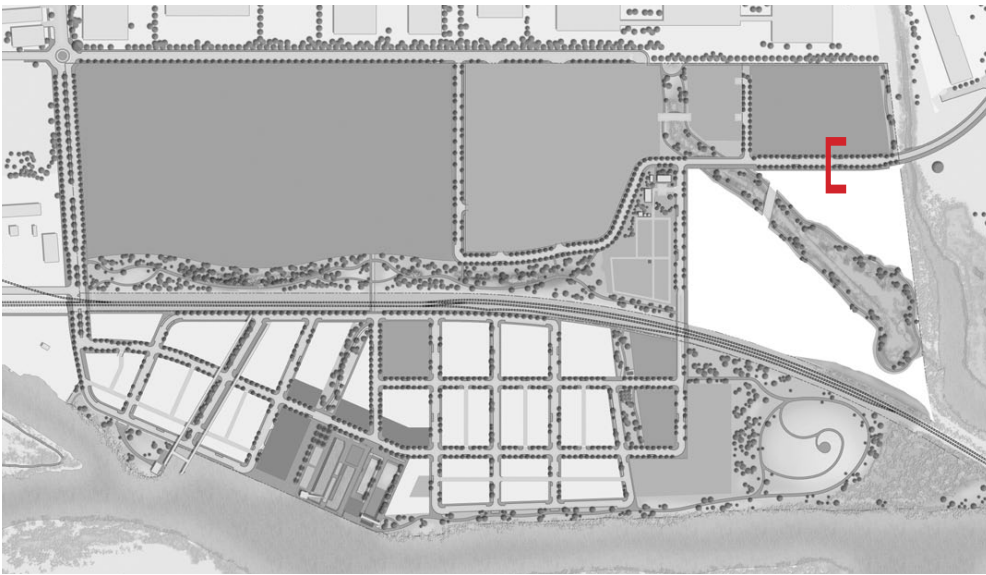
Figure V.2.k - SECTION H



**GENERAL NOTE:**

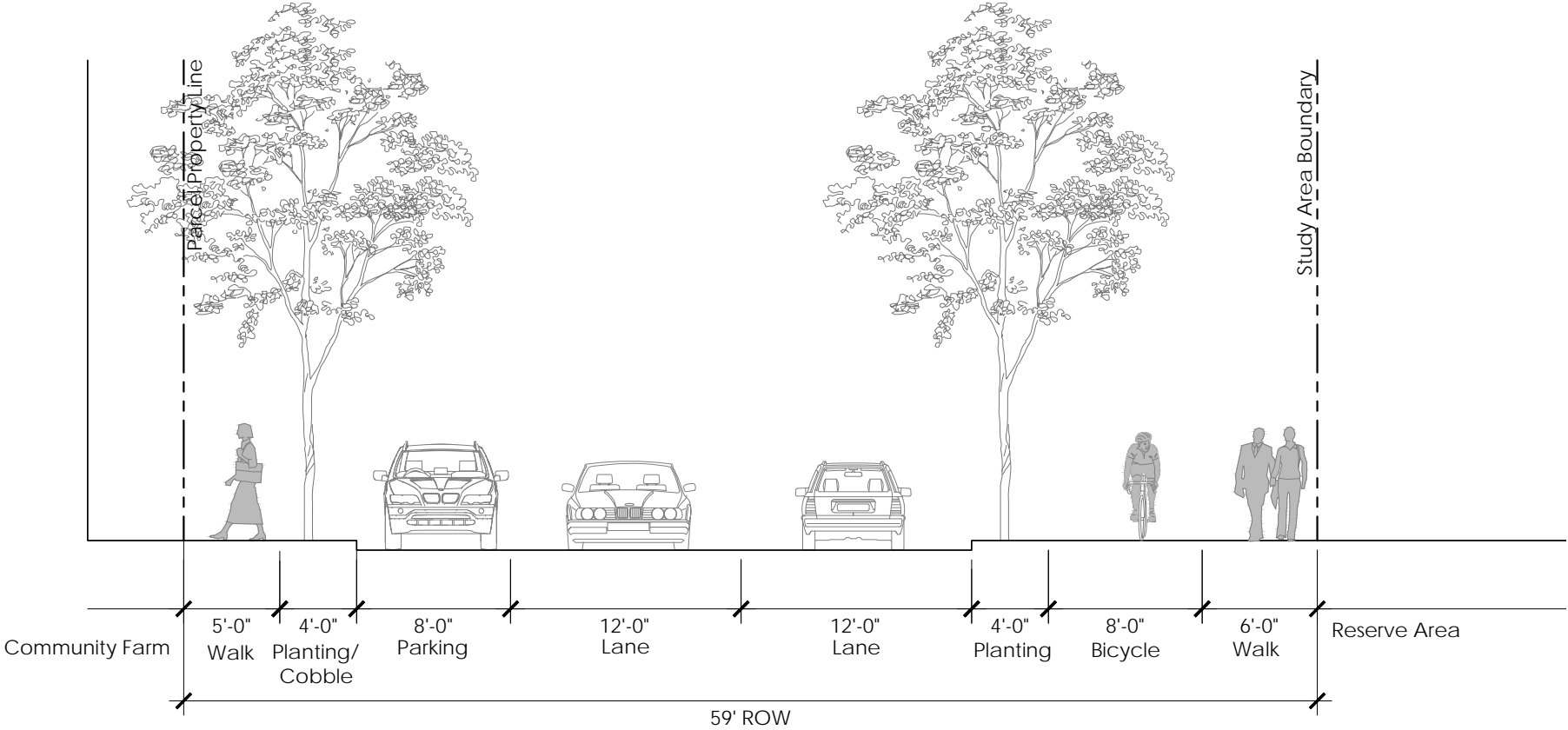
Coordinate street tree locations with all parallel street parking, steps & stoops.

Tree trunk location to be setback a minimum of 24" from face of curb.

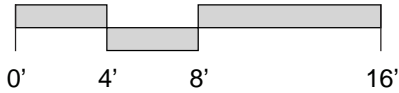


V.2 ROAD & STREET STANDARDS

Figure V.2.I - SECTION I



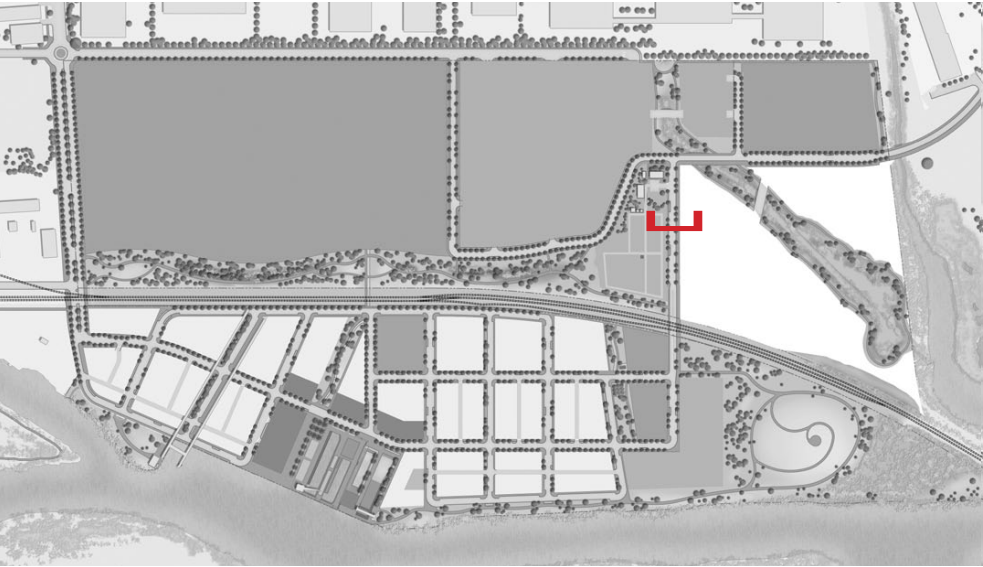
**COLLECTOR ROAD**  
speed limit: 30 mph



**GENERAL NOTE:**

Coordinate street tree locations with all parallel street parking, steps & stoops.

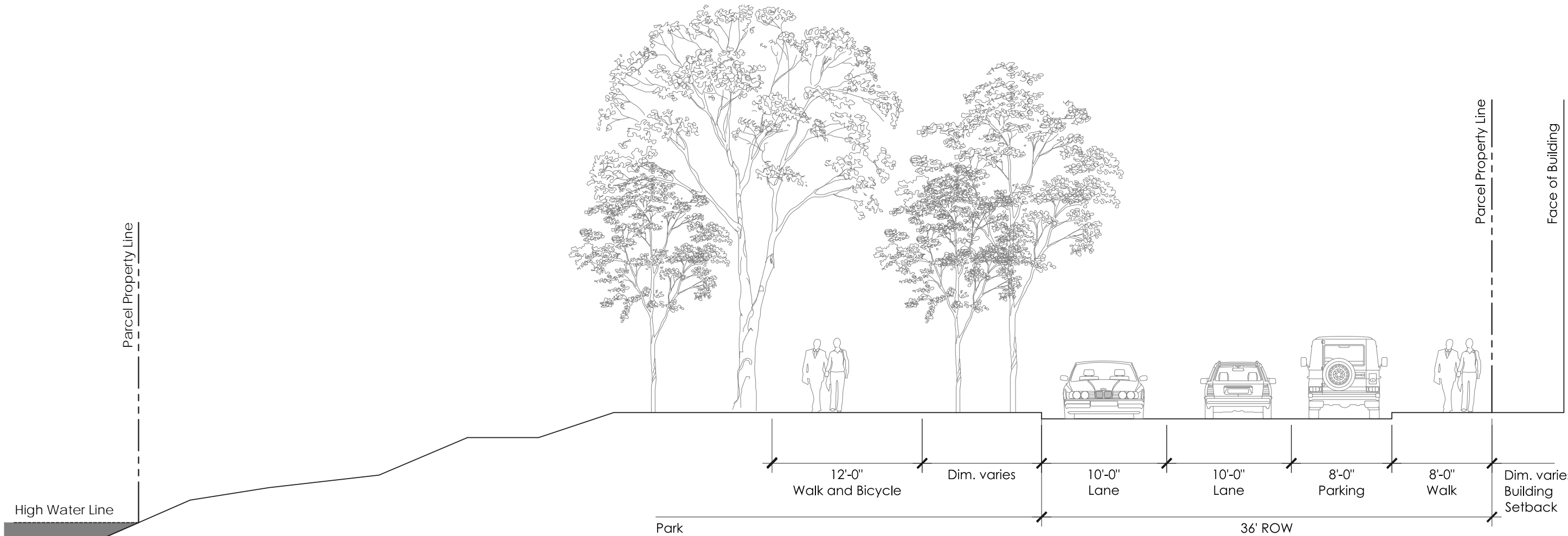
Tree trunk location to be setback a minimum of 24" from face of curb.



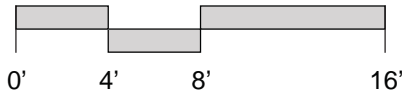


V.2 ROAD & STREET STANDARDS

Figure V.2.m - SECTION J'



**LOCAL ROAD**  
speed limit: 25 mph



**GENERAL NOTE:**

Coordinate street tree locations with all parallel street parking, steps & stoops.

Tree trunk location to be setback a minimum of 24" from face of curb.

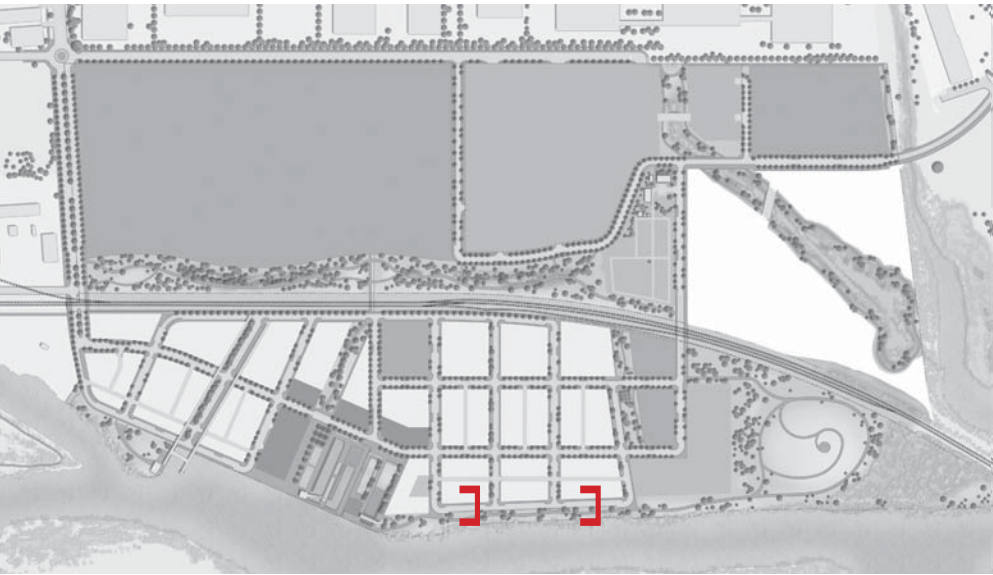
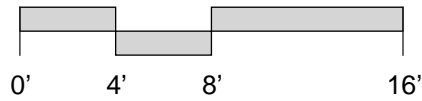
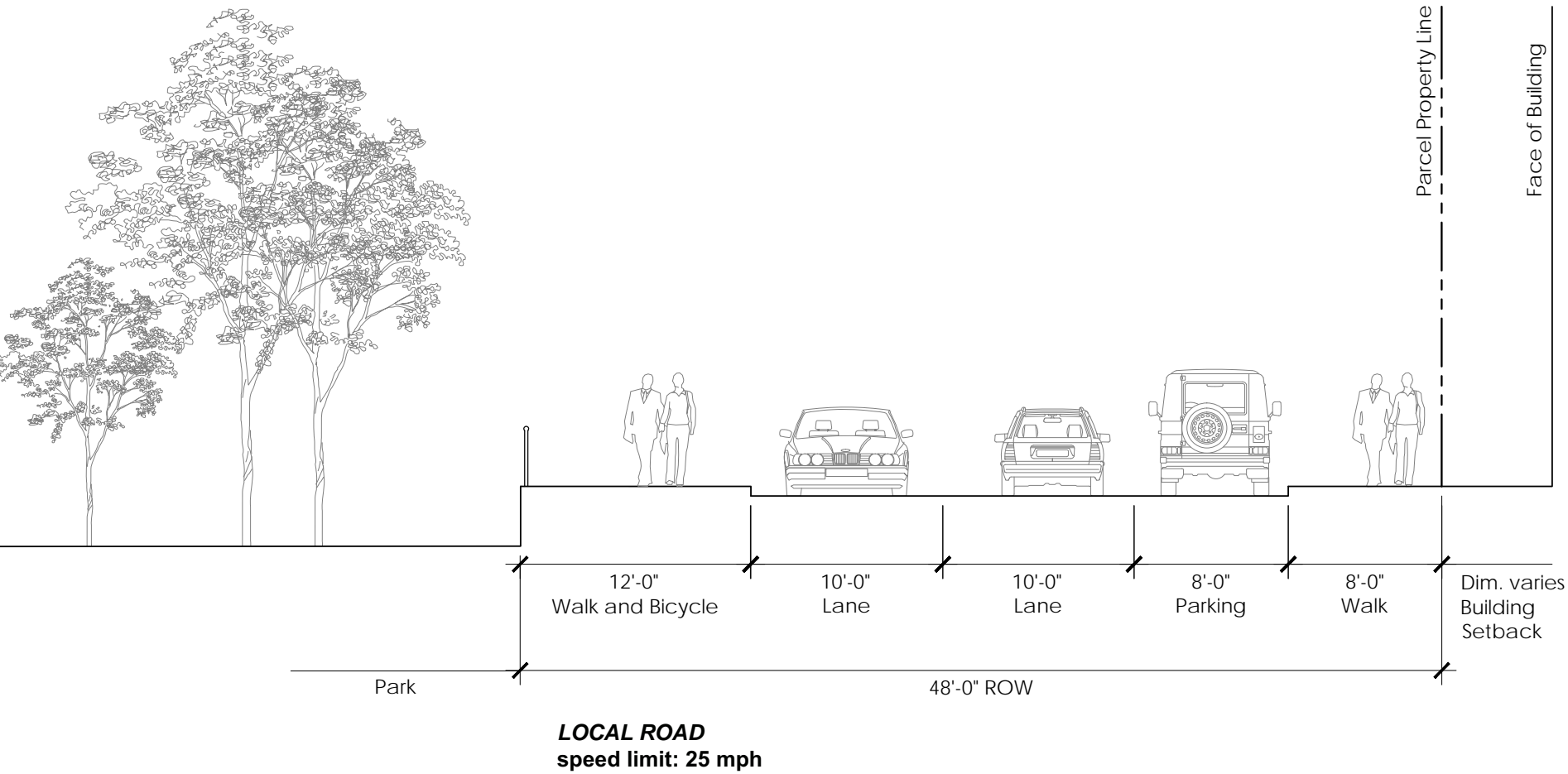


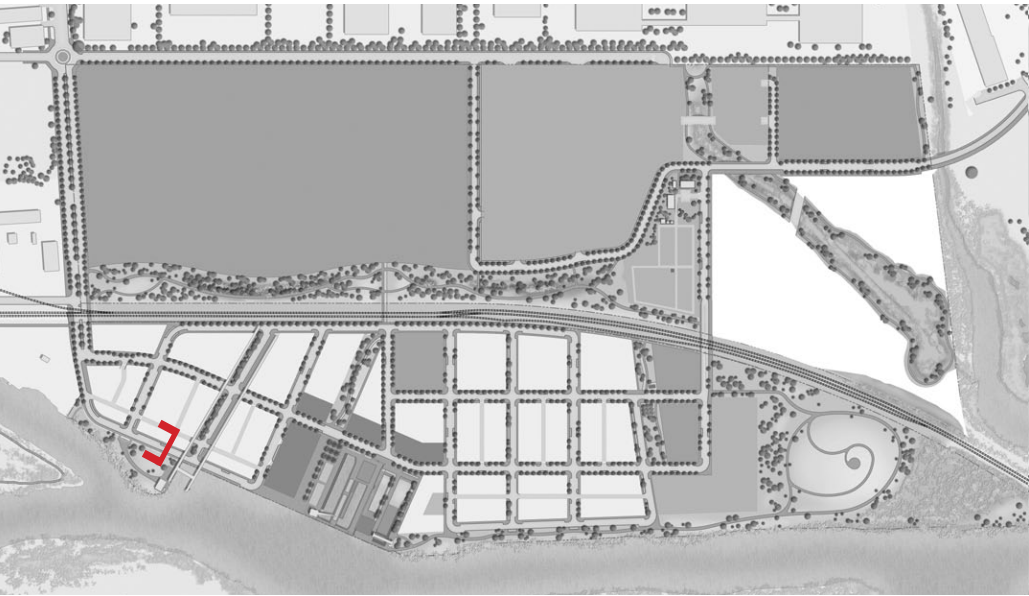
Figure V.2.n - SECTION J2'



**GENERAL NOTE:**

Coordinate street tree locations with all parallel street parking, steps & stoops.

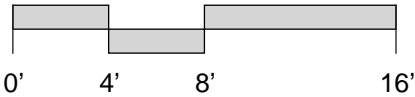
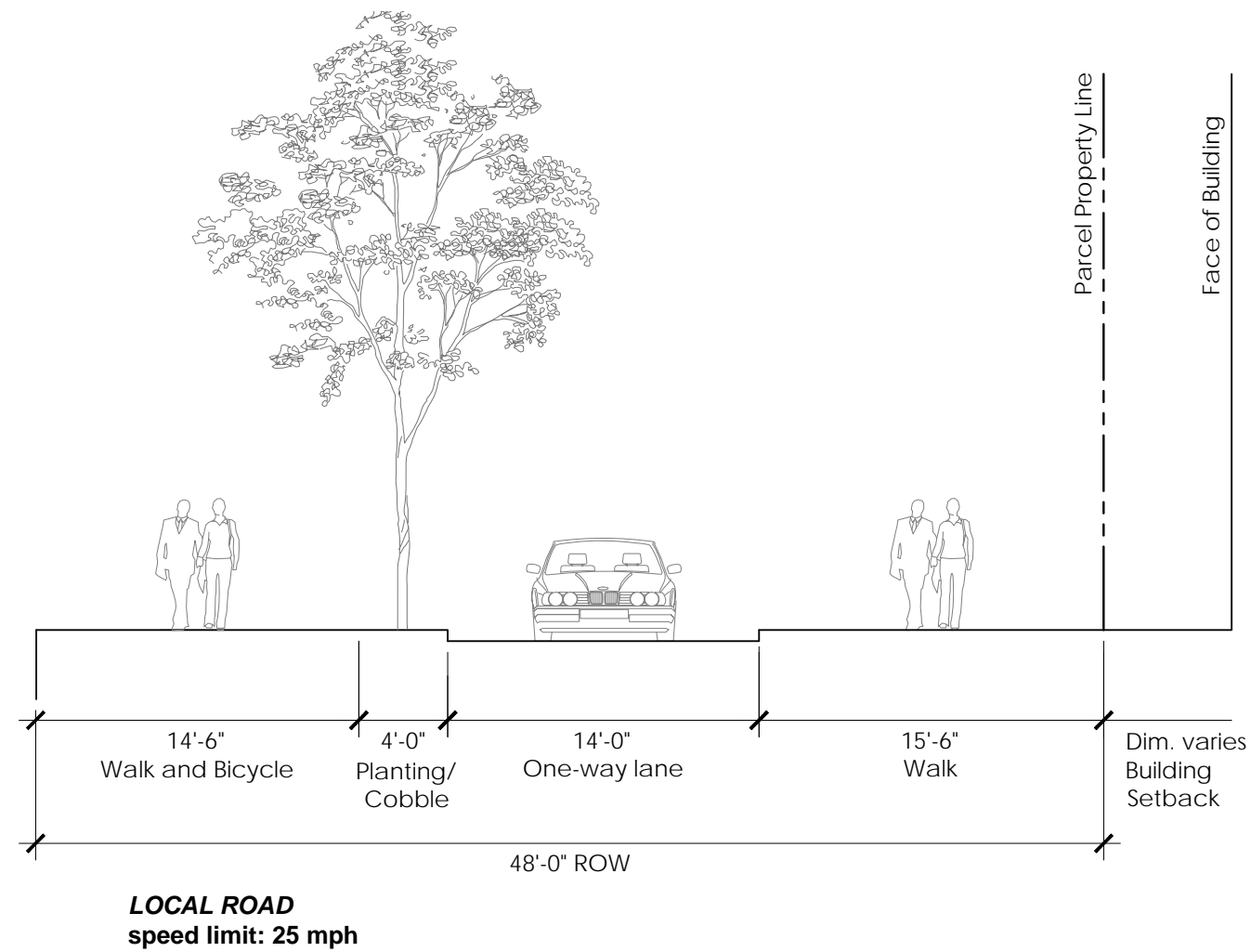
Tree trunk location to be setback a minimum of 24" from face of curb.



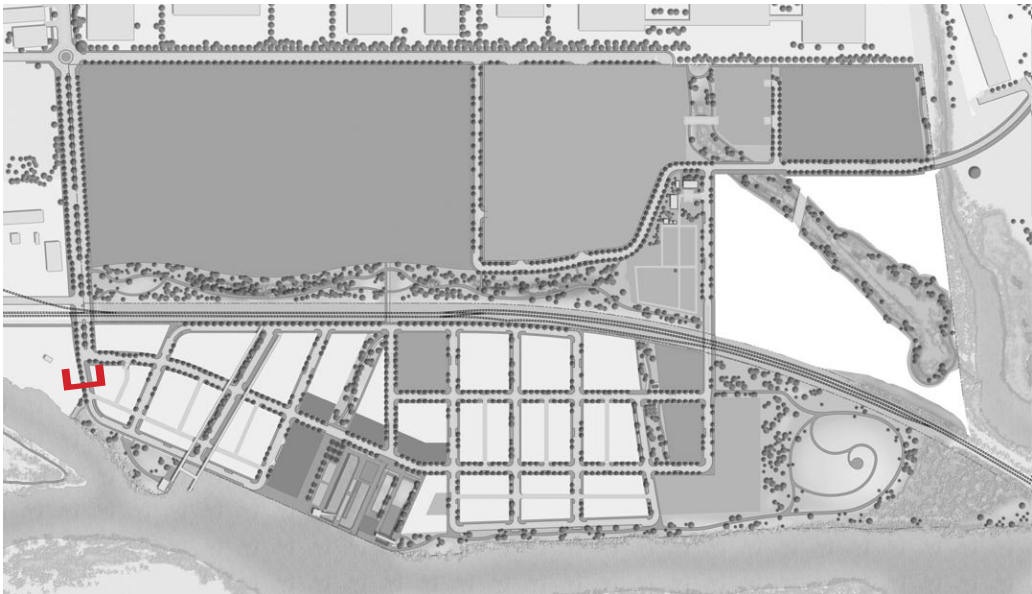


V.2 ROAD & STREET STANDARDS

Figure V.2.o - SECTION J3'

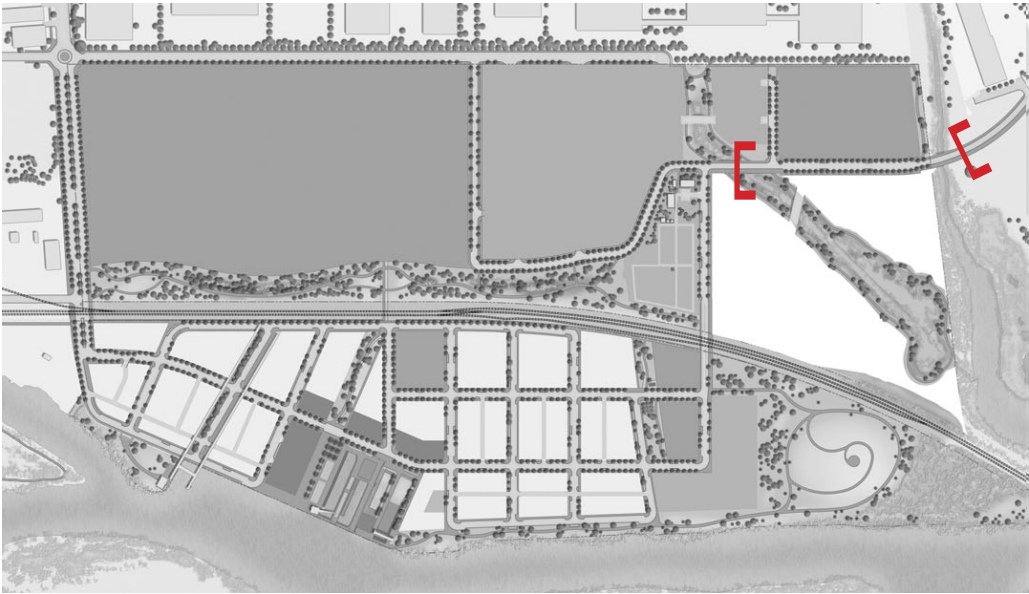
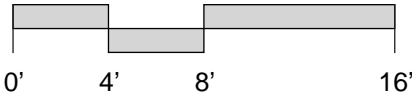
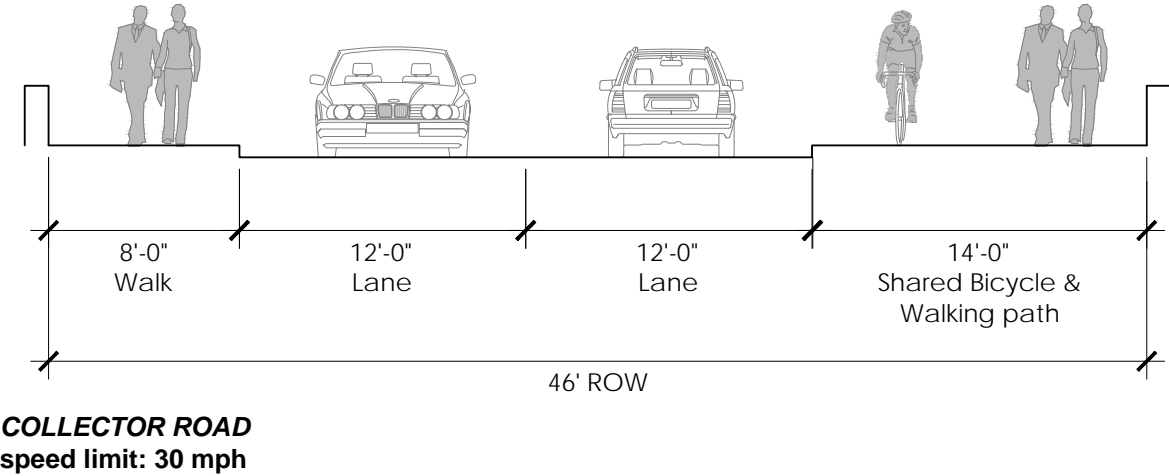


GENERAL NOTE:  
Coordinate street tree locations with all parallel street parking, steps & stoops.  
Tree trunk location to be setback a minimum of 24" from face of curb.



V.2 ROAD & STREET STANDARDS

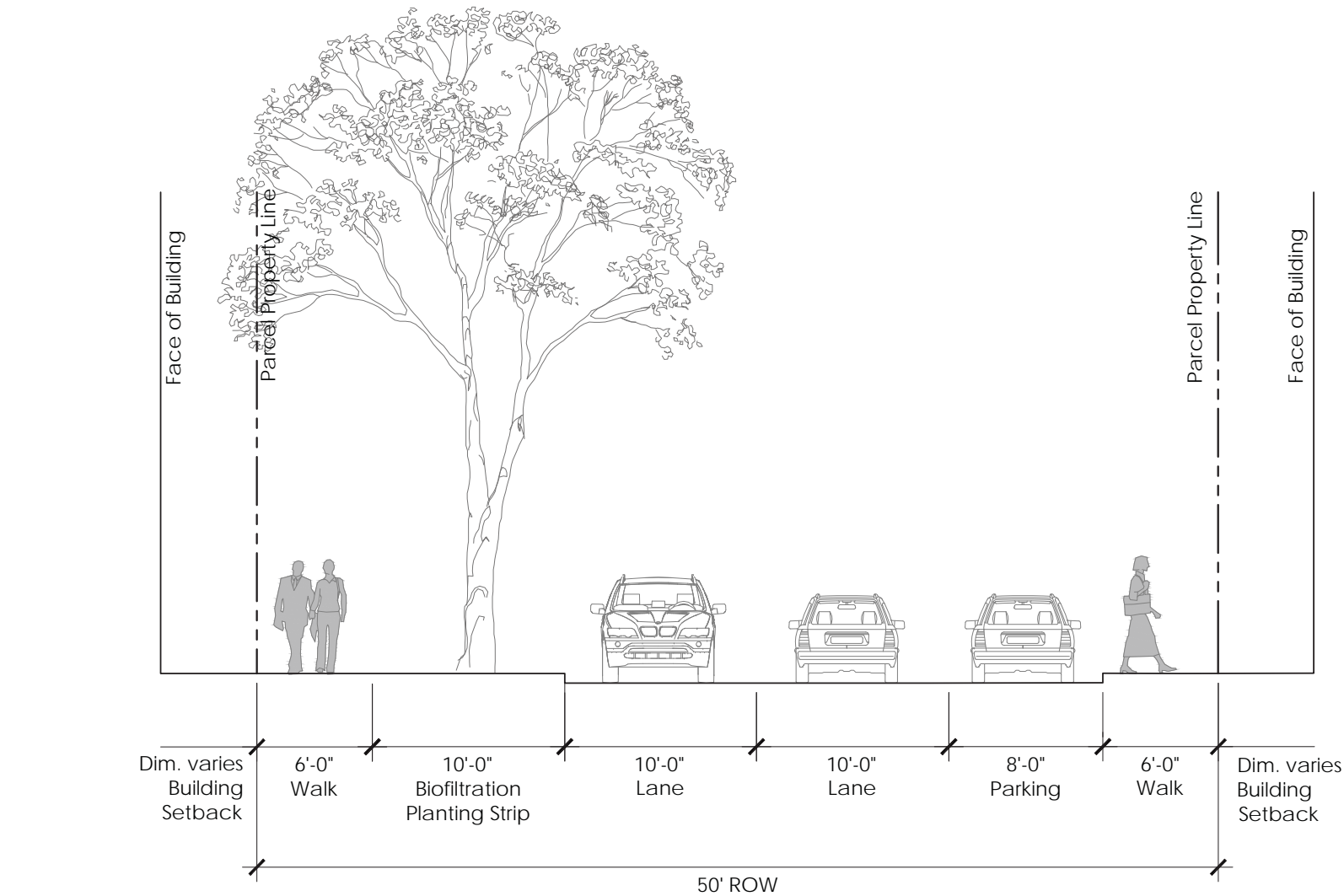
Figure V.2.p - SECTION K



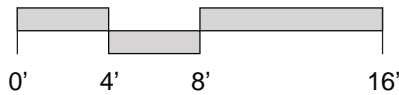


V.2 ROAD & STREET STANDARDS

Figure V.2.q - SECTION L



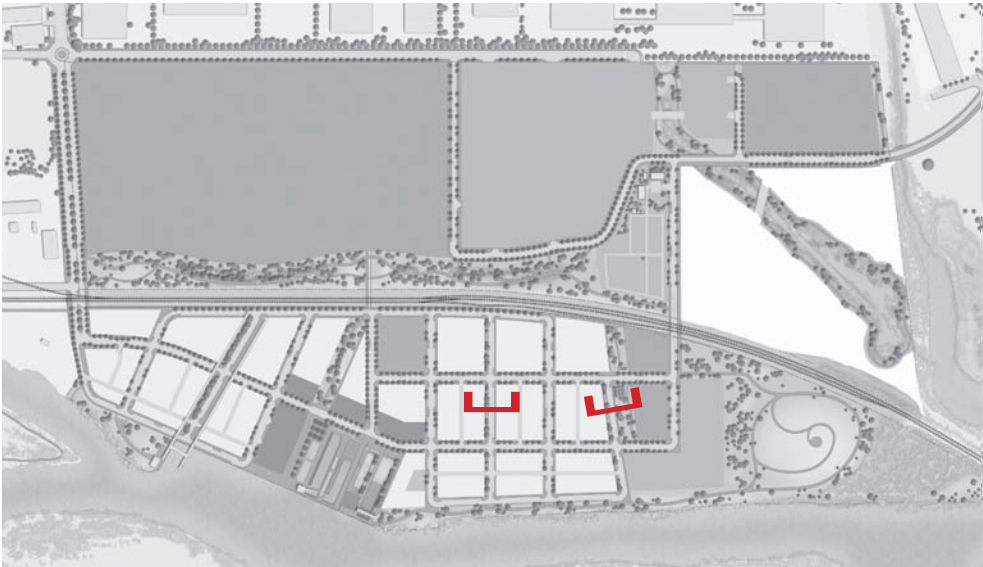
**LOCAL ROAD**  
speed limit: 25 mph



**GENERAL NOTE:**

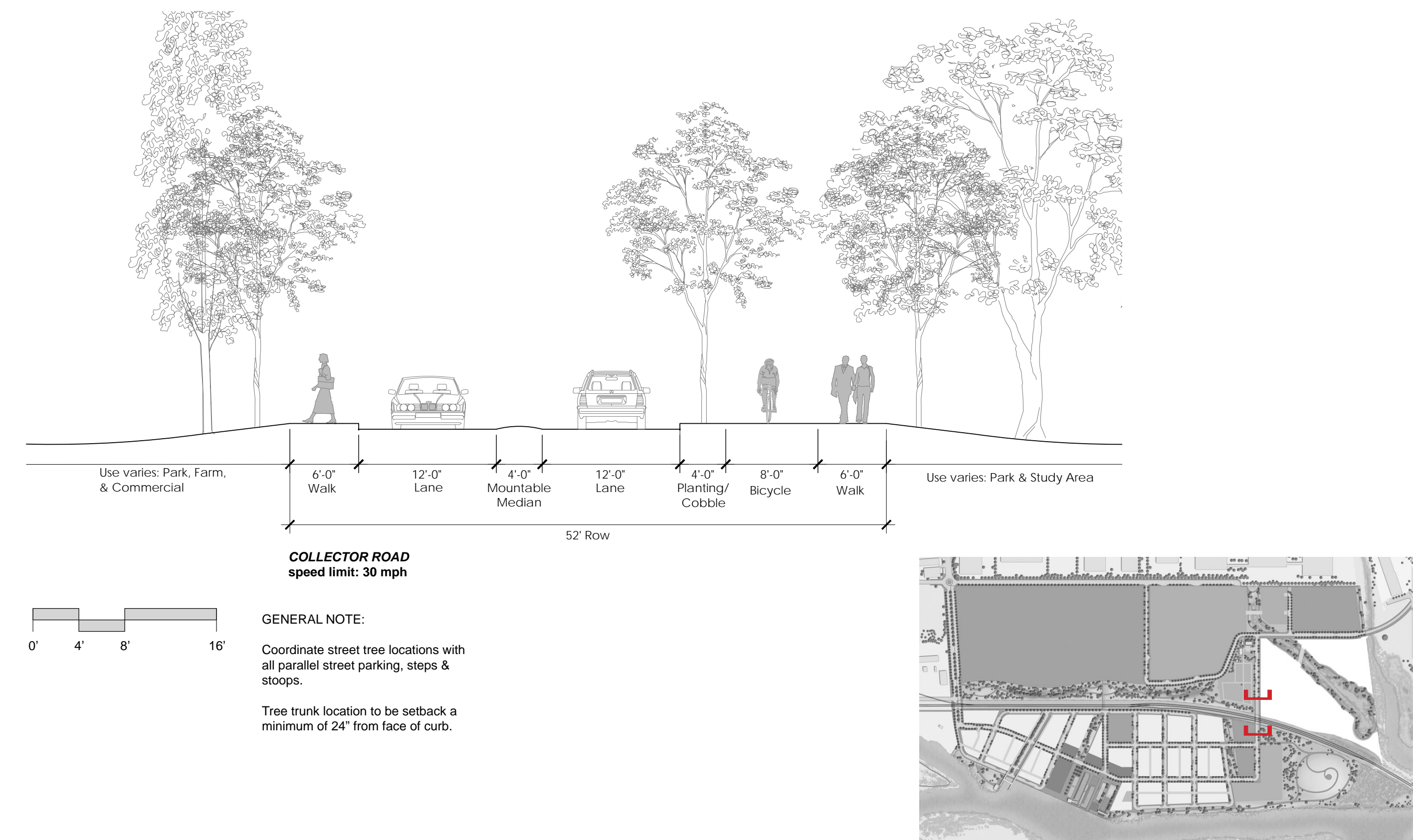
Coordinate street tree locations with all parallel street parking, steps & stoops.

Tree trunk location to be setback a minimum of 24" from face of curb.



V.2 ROAD & STREET STANDARDS

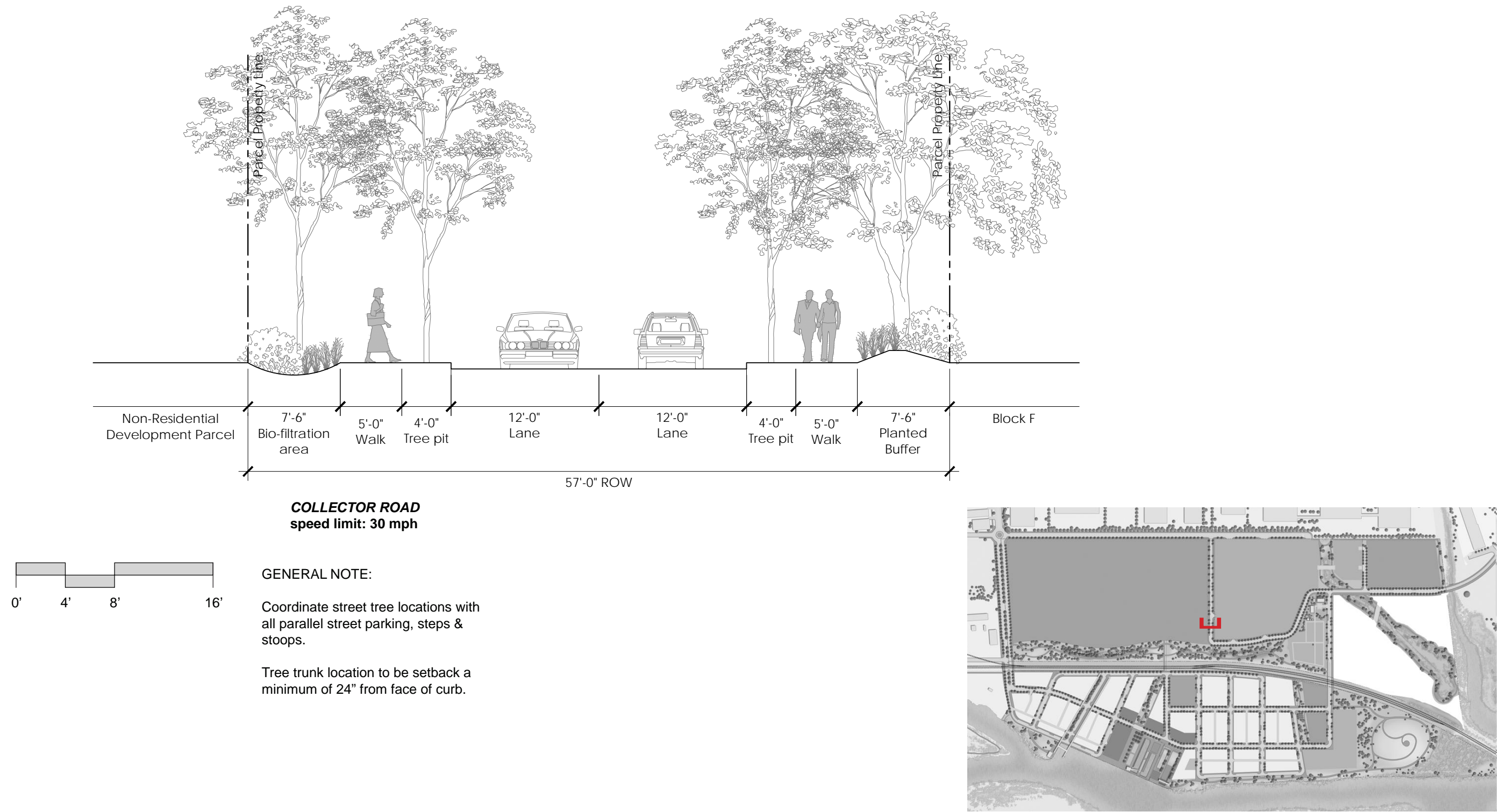
Figure V.2.r - SECTION N





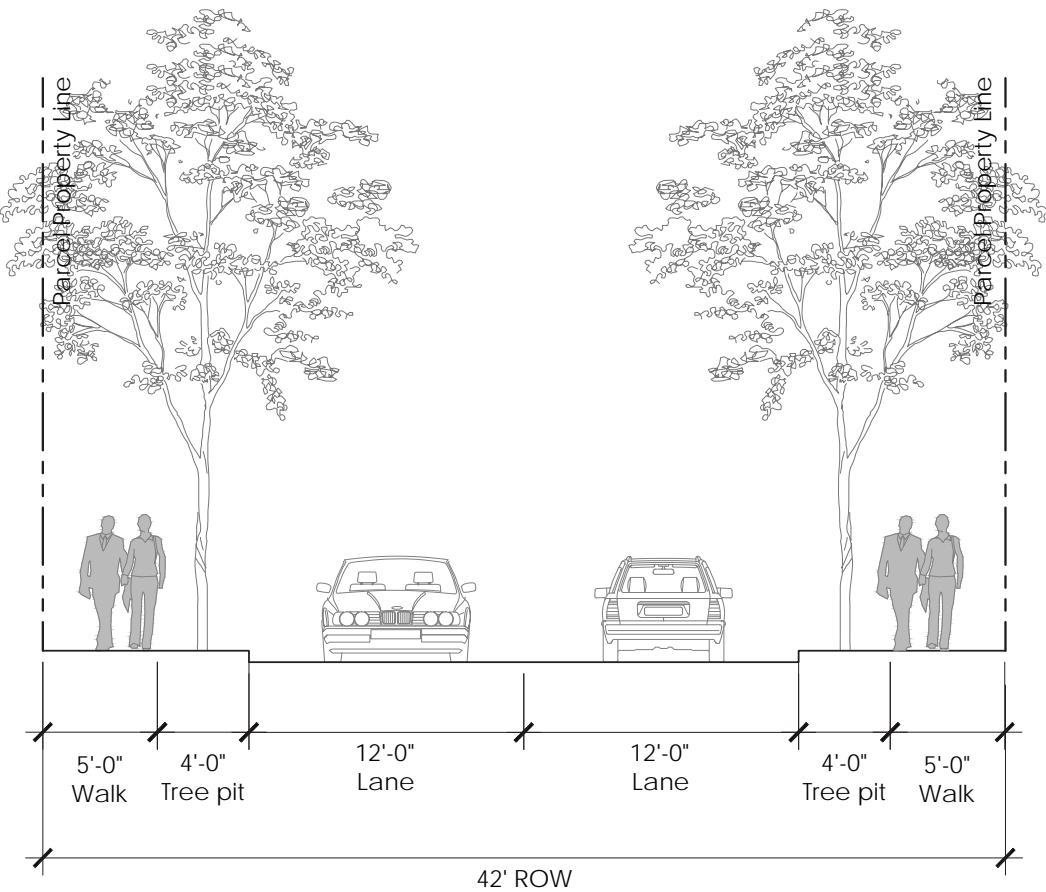
V.2 ROAD & STREET STANDARDS

Figure V.2.s - SECTION O

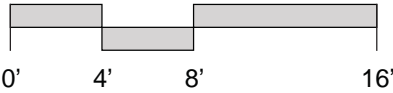


V.2 ROAD & STREET STANDARDS

Figure V.2.t - SECTION P



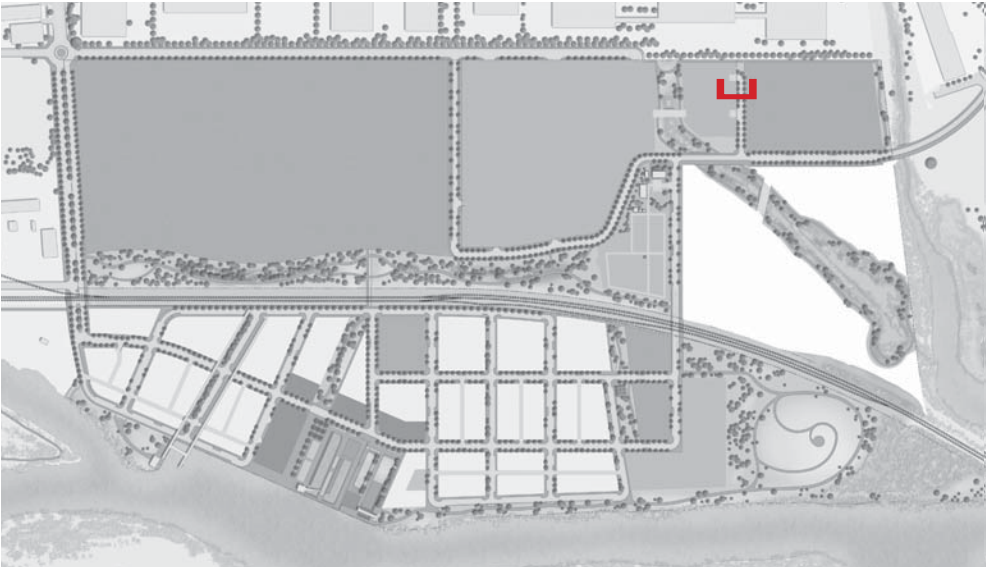
**LOCAL ROAD**  
speed limit: 25 mph



**GENERAL NOTE:**

Coordinate street tree locations with all parallel street parking, steps & stoops.

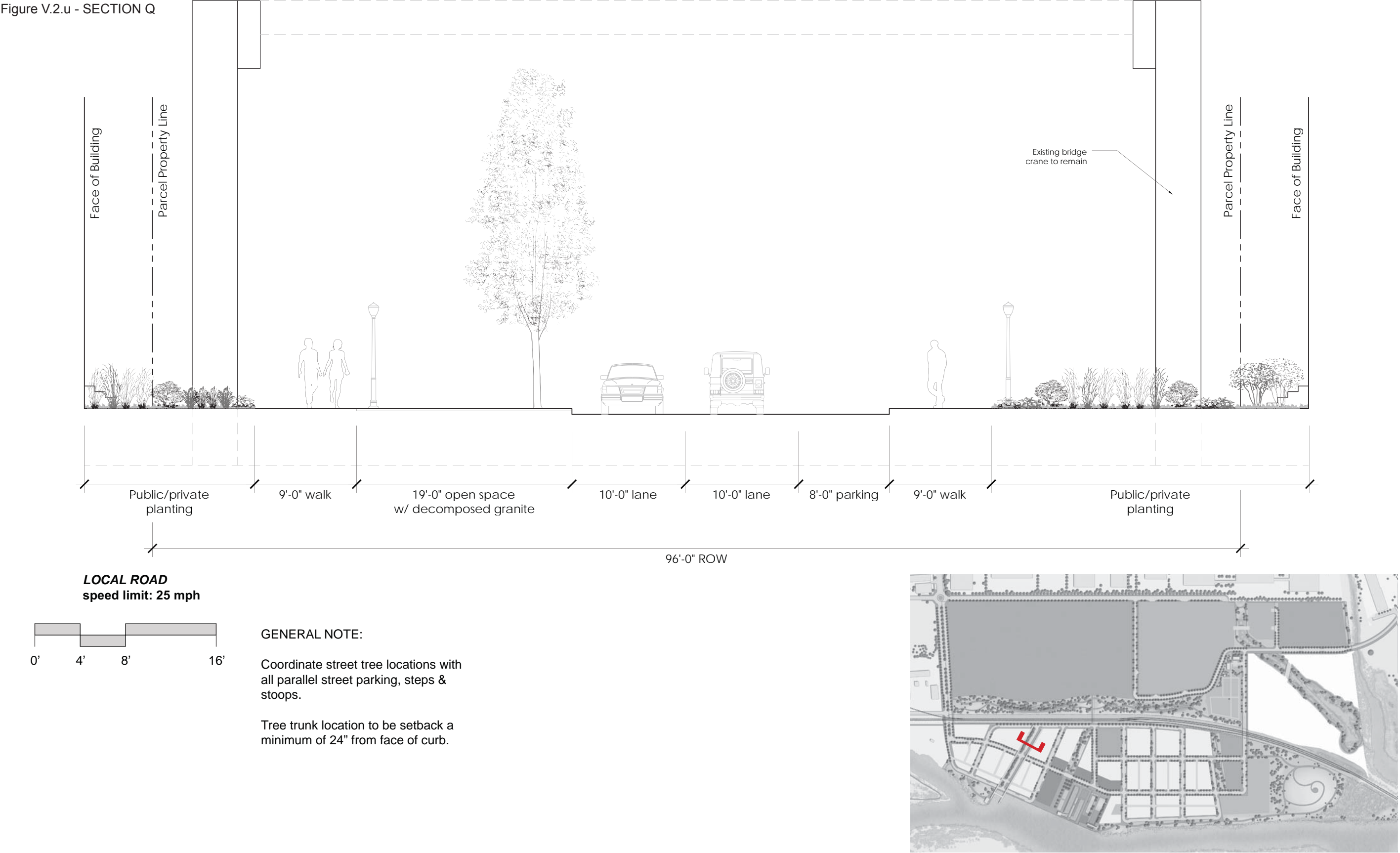
Tree trunk location to be setback a minimum of 24" from face of curb.





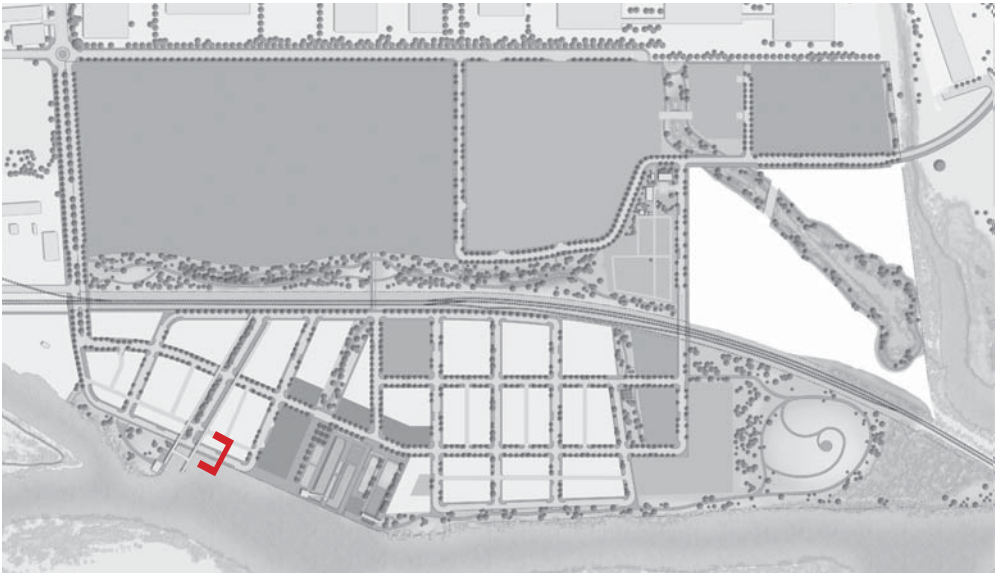
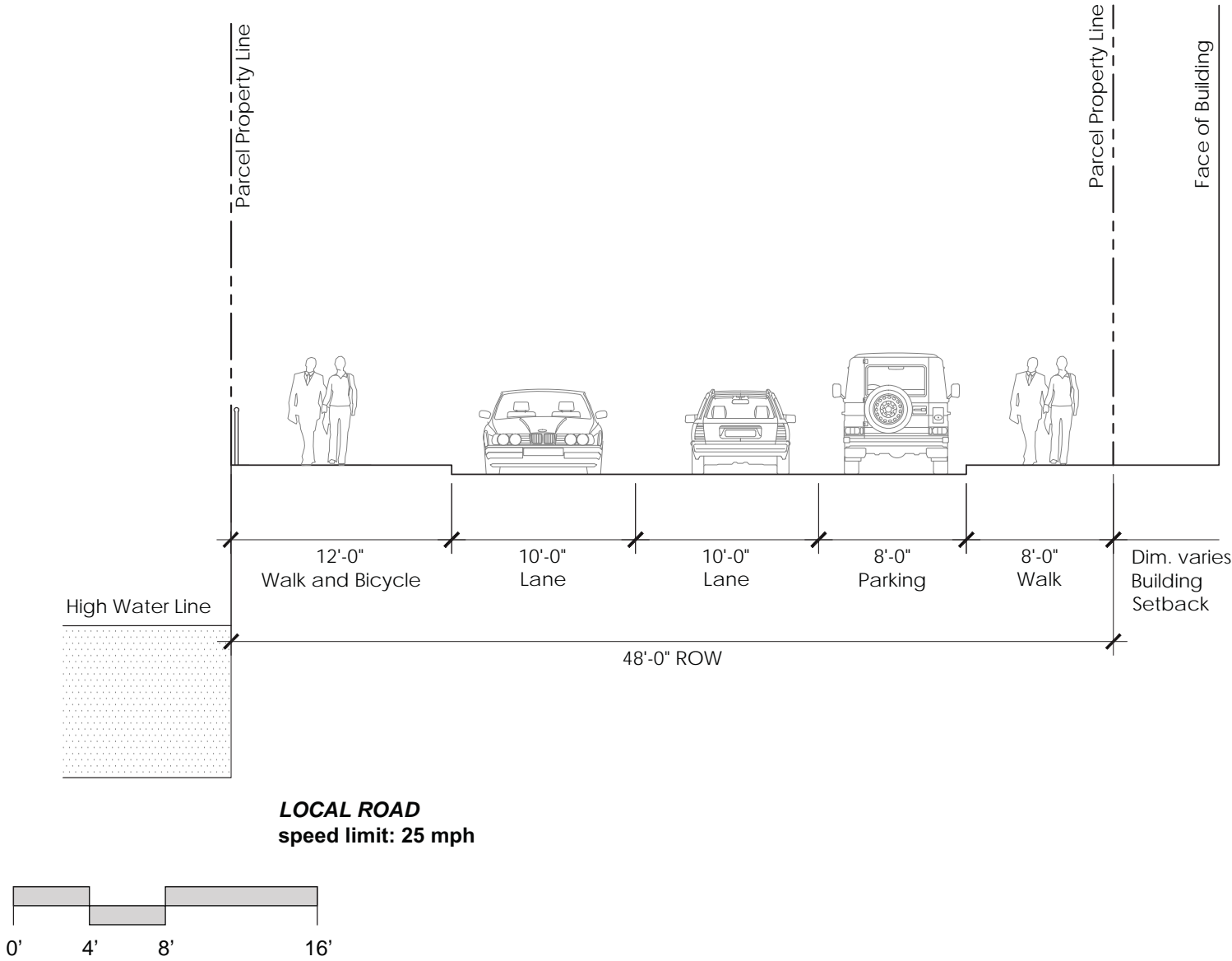
V.2 ROAD & STREET STANDARDS

Figure V.2.u - SECTION Q



V.2 ROAD & STREET STANDARDS

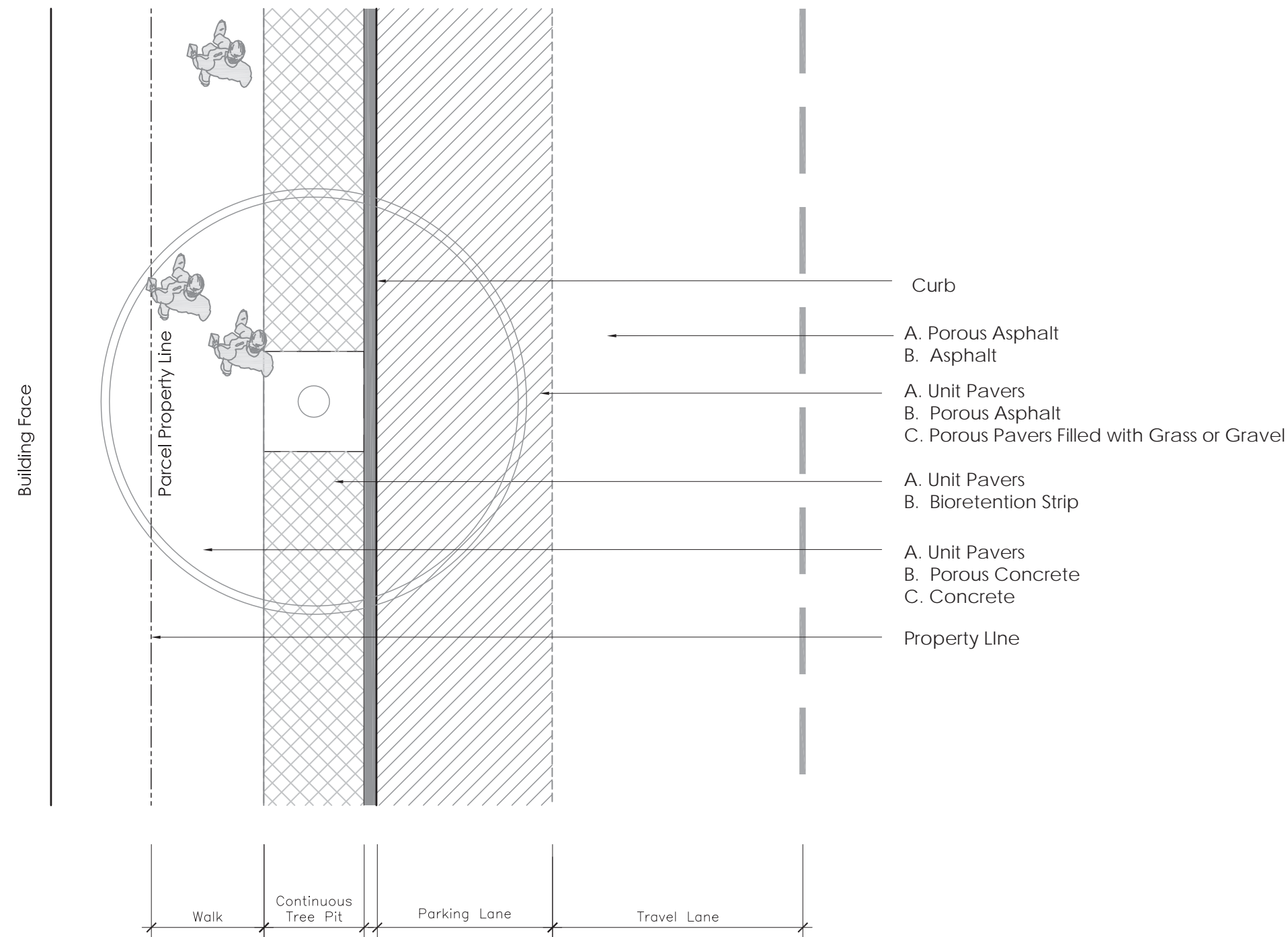
Figure V.2.v - SECTION R'





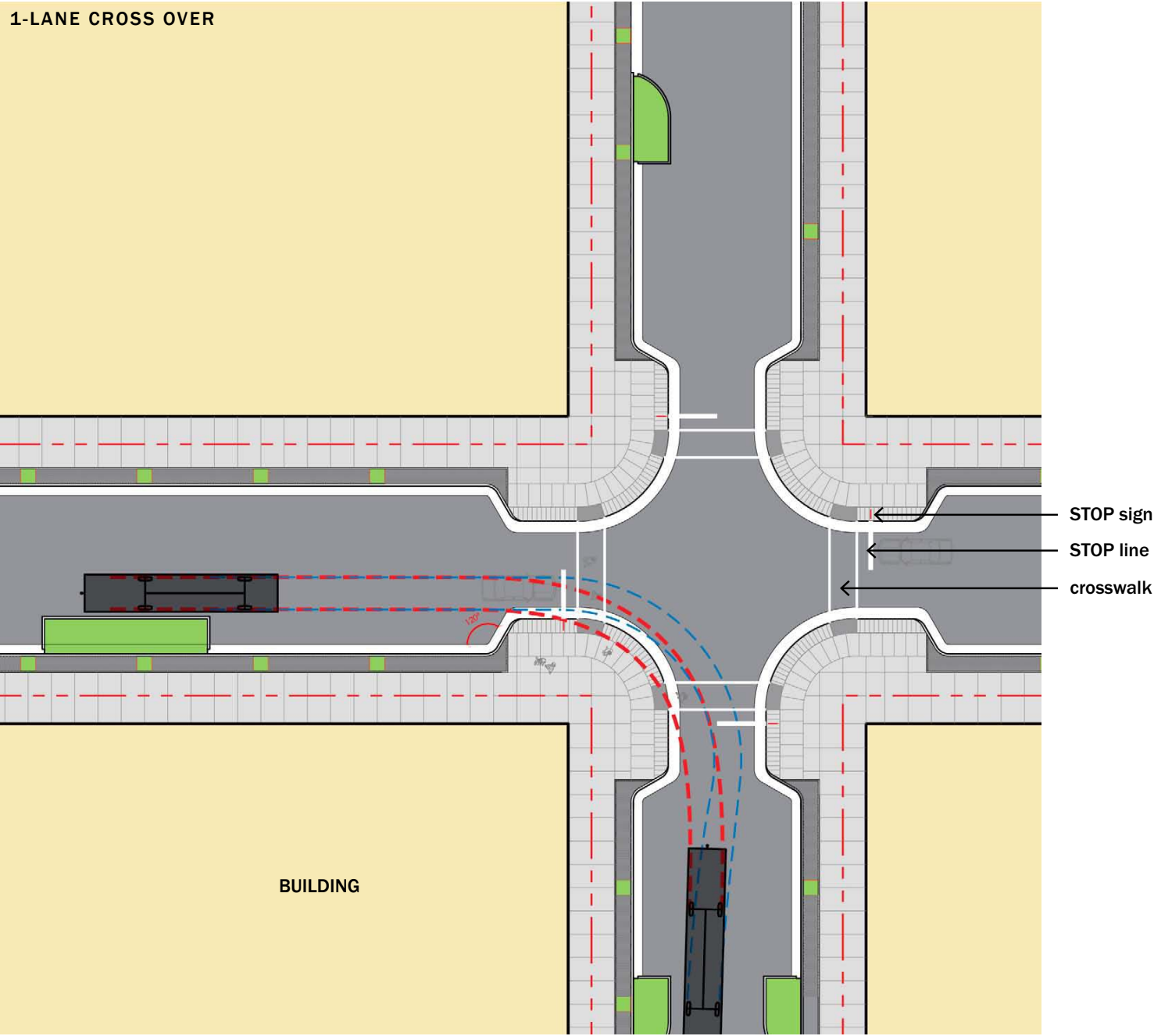
V.2 ROAD & STREET STANDARDS

Figure V.2.w - MATERIAL OPTIONS



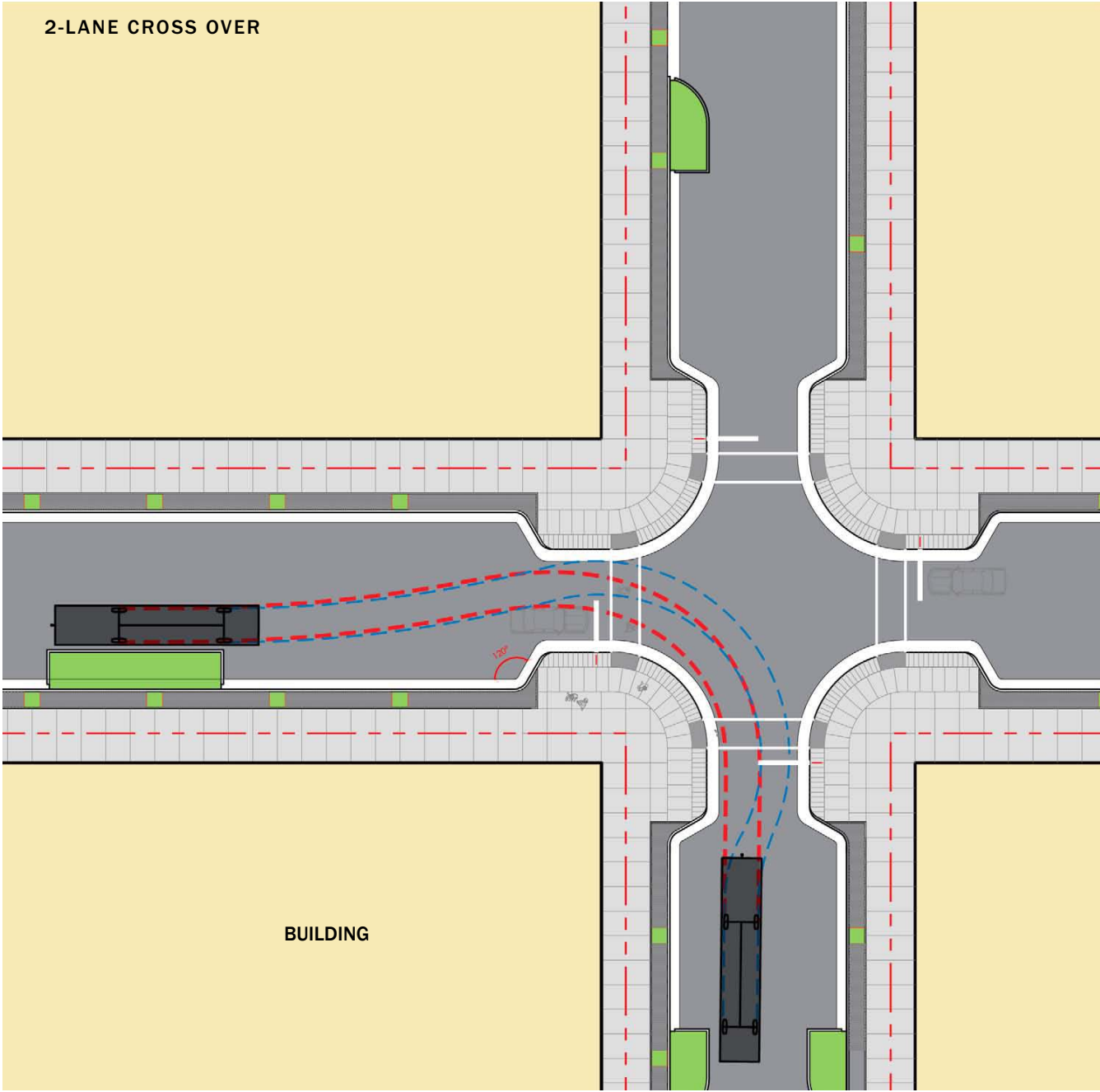
V.2 ROAD & STREET STANDARDS

Figure V.2.x - INTERSECTIONS - OVERALL PLANS



**LEGEND**

	parcel boundary		building
	fire truck rear tire		gutter & curb
	fire truck front tire		road paving
	light tone paving		
	medium tone paving		
	dark tone paving		



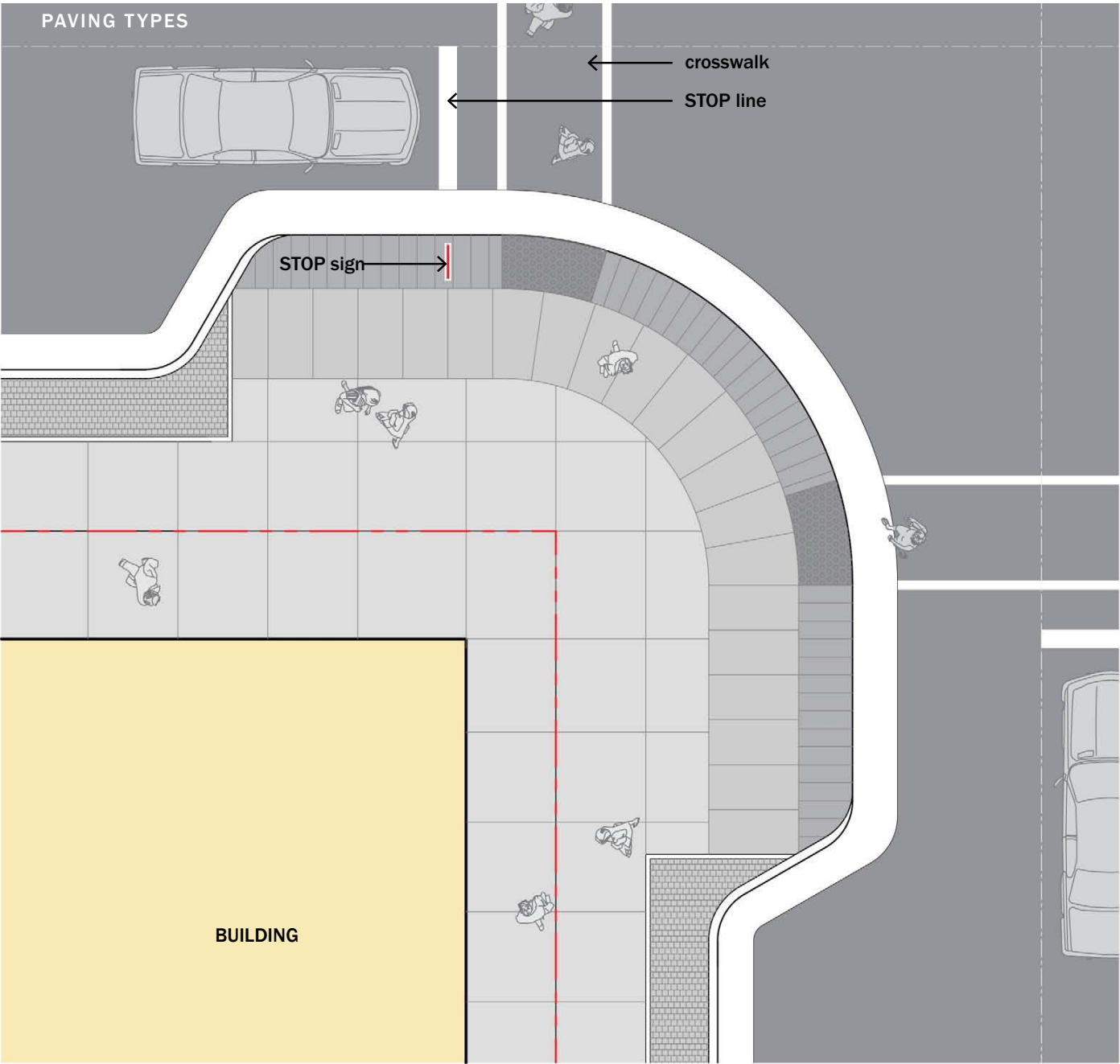
**NOTES:**  
Speed limit for all local streets is 25 mph.

Intersections have been designed to accomodate fire engine maneuvers py providing a flush curb in conjunction with a different colored/textured sidewalk to allow emergency vehicles to drive in this area. Additionally the curb transitions from the parking bays to the curb returns were angled and the corners rounded to allow for a larger sweep fire engine.



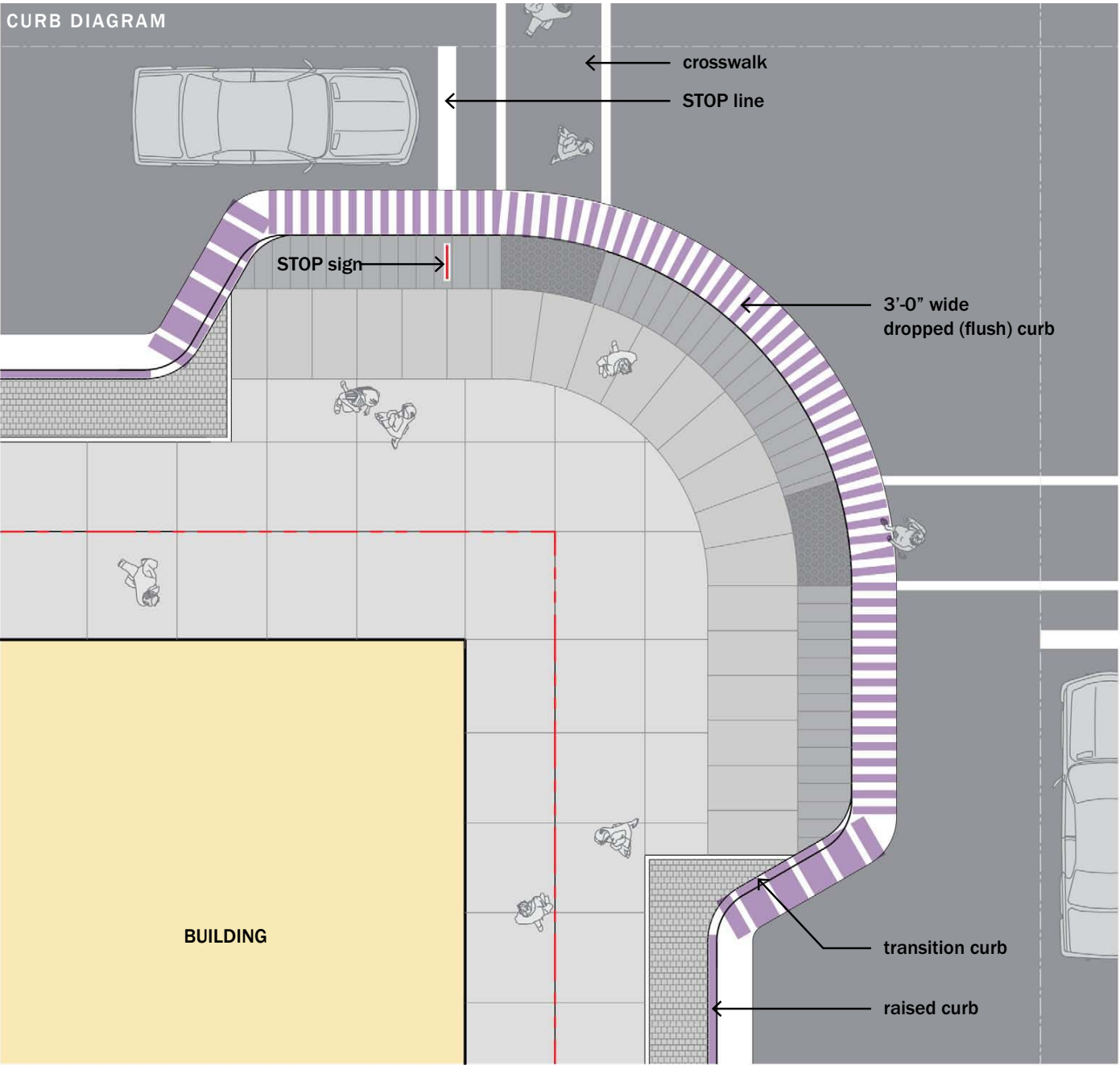
V.2 ROAD & STREET STANDARDS

FIGURE V.2.y - INTERSECTIONS - PAVING ENLARGEMENTS



LEGEND

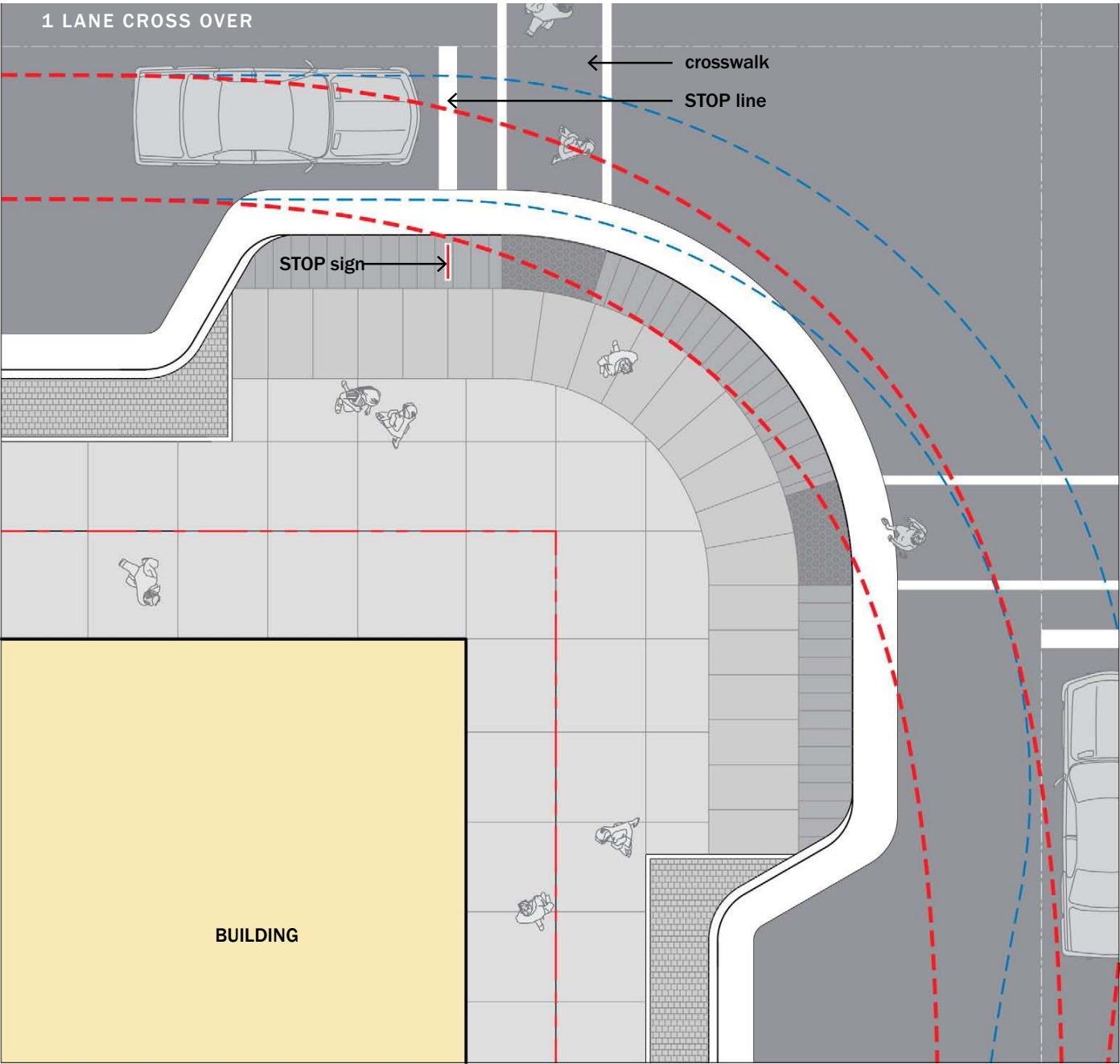
	parcel boundary		building		dropped curb
	fire truck rear tire		gutter & curb		transition curb
	fire truck front tire		road paving		raised curb
	light tone paving				
	medium tone paving				
	dark tone paving				



NOTES:  
Speed limit for all local streets is 25 mph.

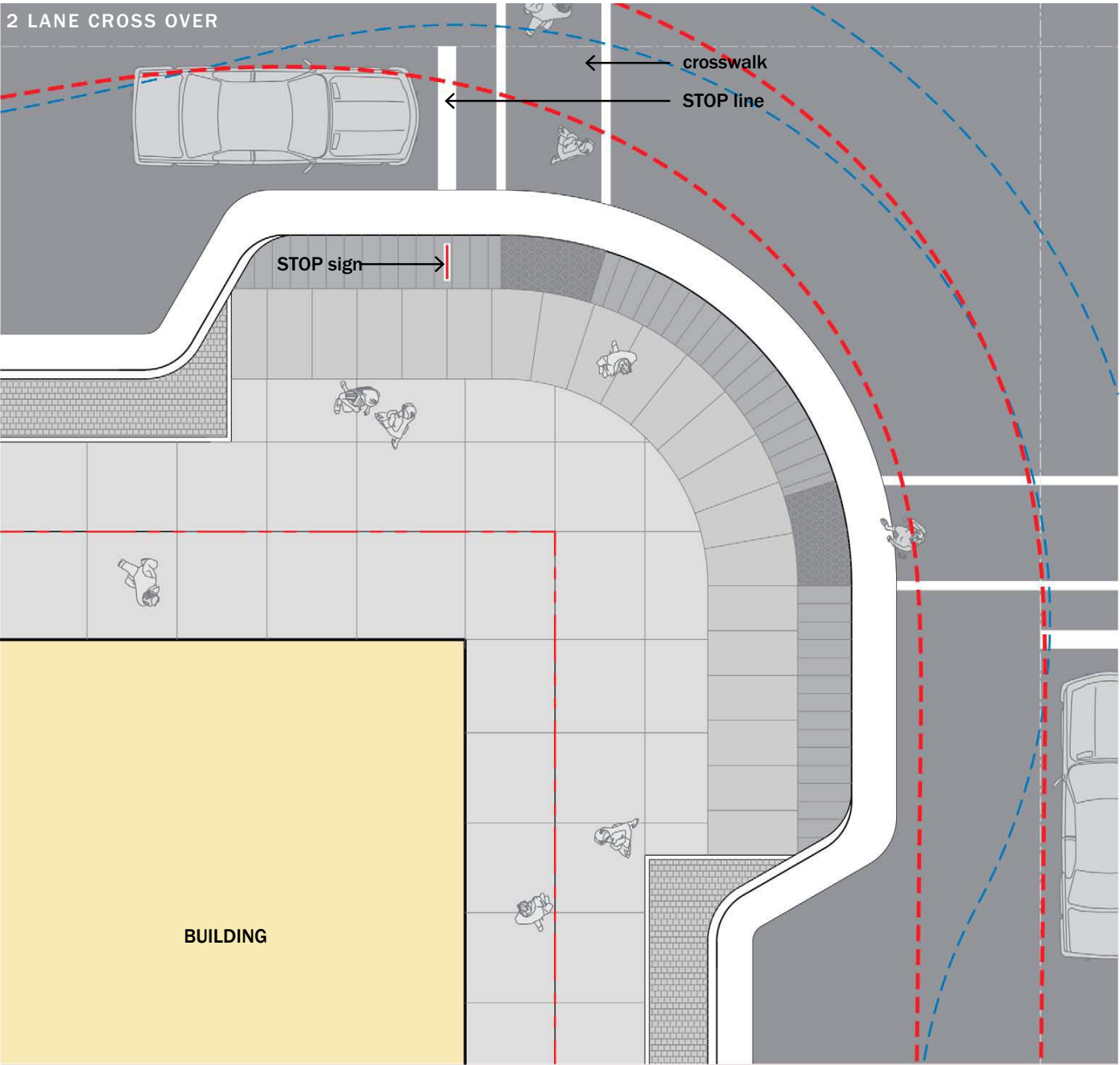
V.2 ROAD & STREET STANDARDS

Figure V.2.z - INTERSECTIONS - PAVING ENLARGEMENTS



**LEGEND**

	parcel boundary		building
	fire truck rear tire		gutter & curb
	fire truck front tire		road paving
	light tone paving		
	medium tone paving		
	dark tone paving		

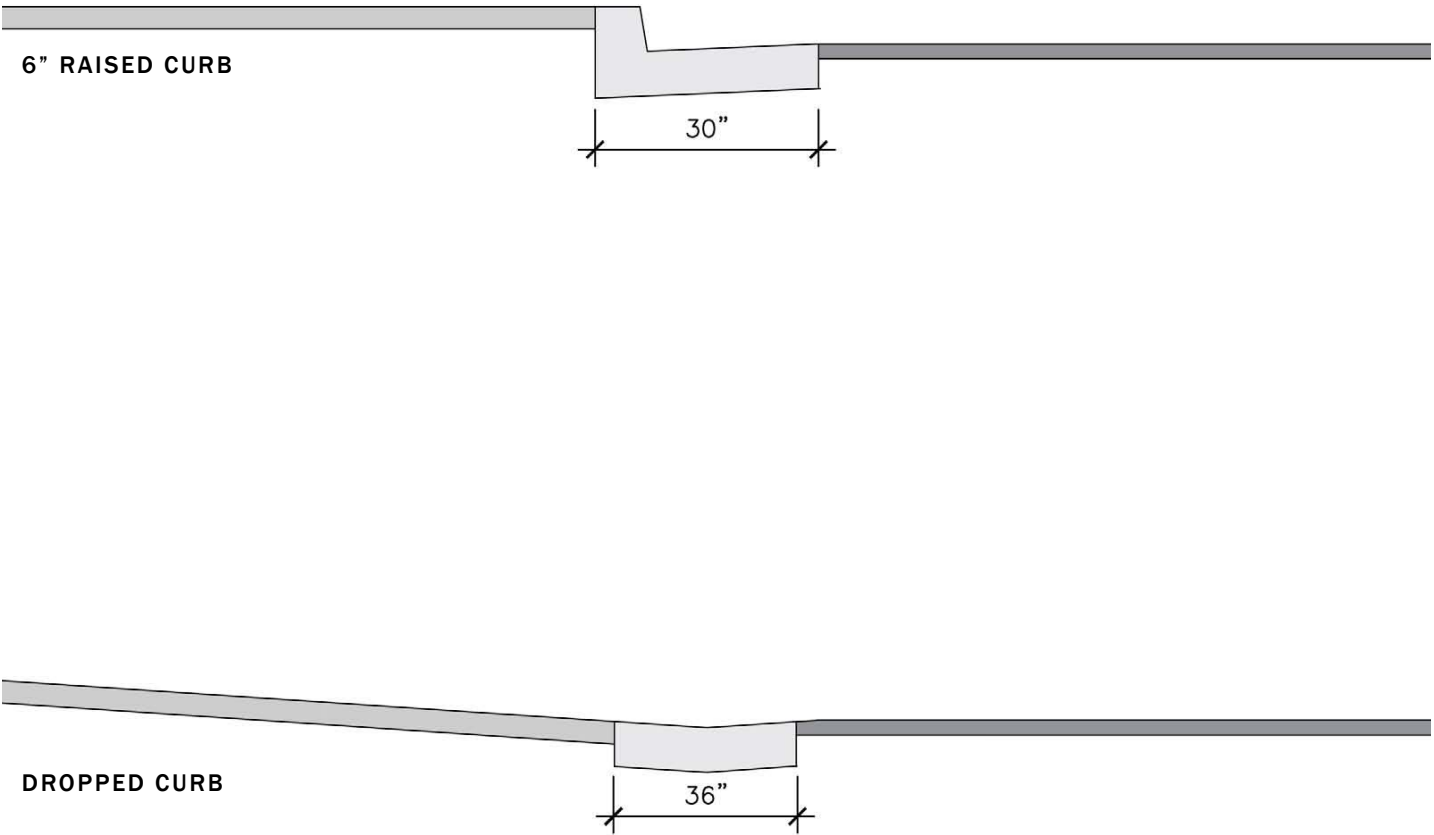


**NOTES:**  
Speed limit for all local streets is 25 mph.



V.2 ROAD & STREET STANDARDS

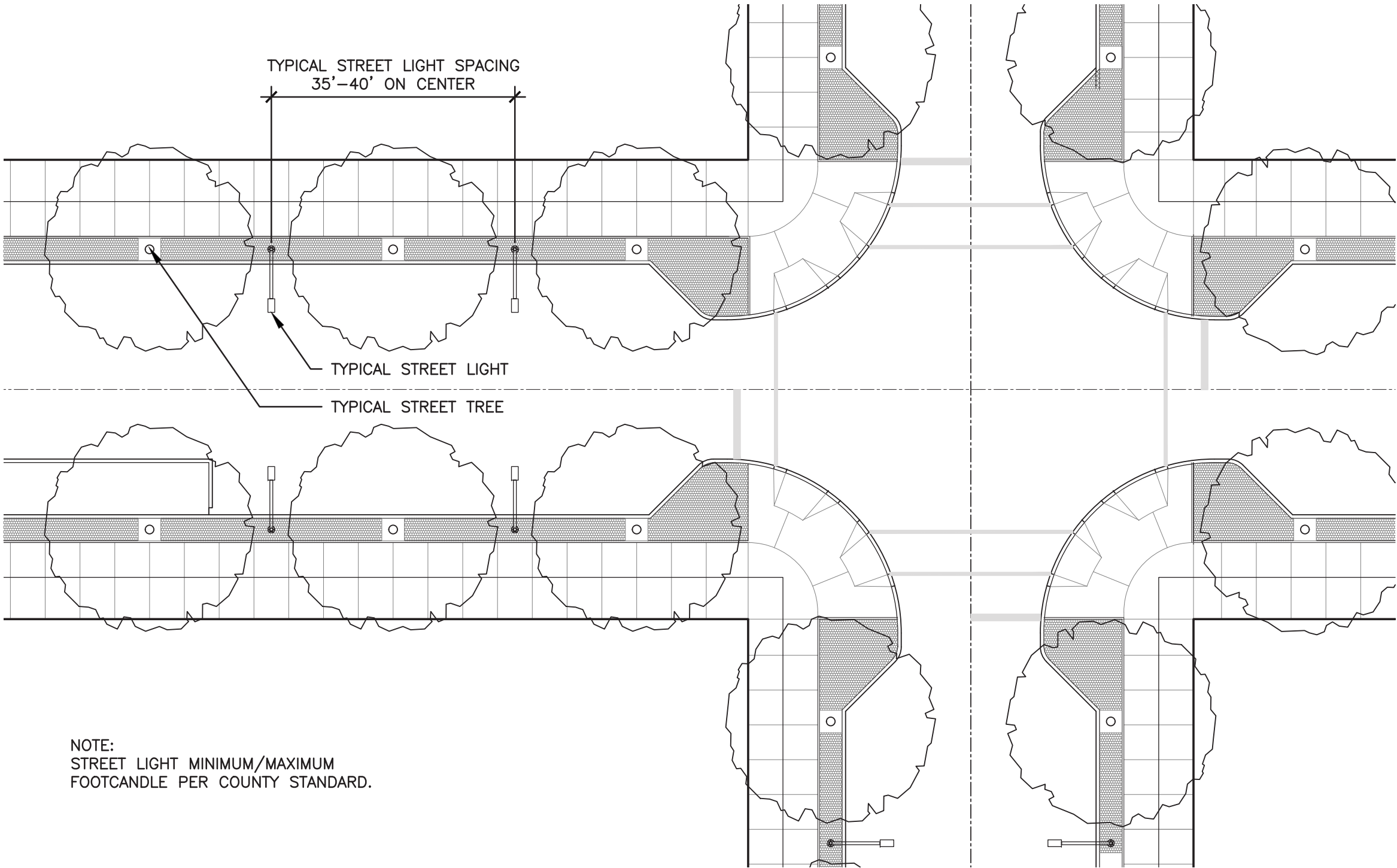
Figure V.2.aa - INTERSECTIONS - CURB SECTIONS



LEGEND

<div></div>	light tone paving		
<div></div>	medium tone paving	<div></div>	gutter & curb
<div></div>	dark tone paving	<div></div>	road paving

Figure V.2.ab - STREET LIGHTS





V.3 KAISER ROAD IMPROVEMENTS

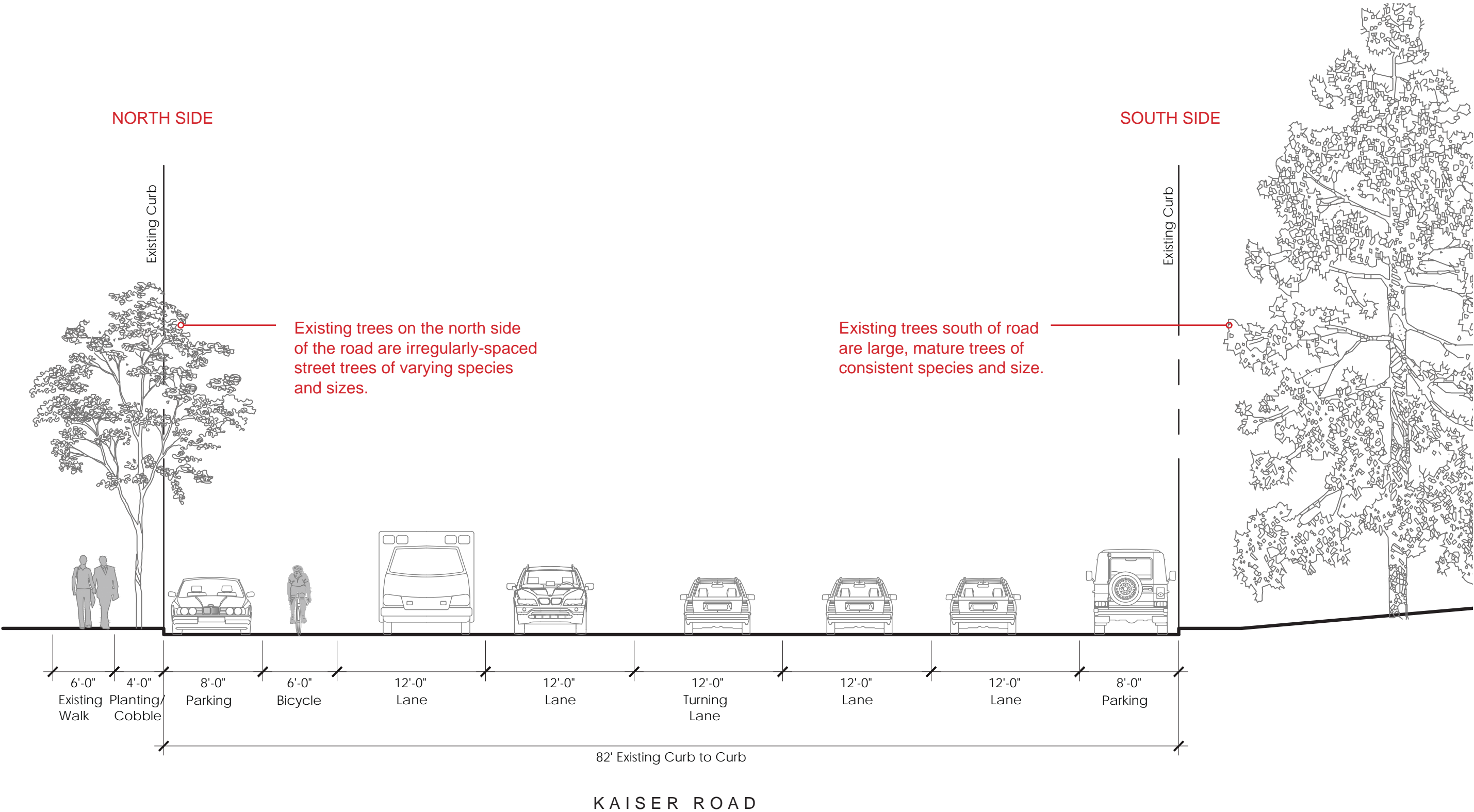
Improvements to Kaiser Road will be undertaken to provide appropriate connections to existing utilities and infrastructure. Design and construction will adhere to common industry practices and applicable standards of the City of Napa and Napa County.

For the Kaiser Road segment between Basalt Road and Napa Vallejo Highway (Hwy 221), the Kaiser Road improvements consist of retaining the existing curbs and planting medium-sized trees on both the north and south sides of the road, as well as potential improvements where Kaiser Rd. intersects Corporate Drive. The trees would be planted approximately 30' to 25' apart in the existing soils. Existing street trees on the north side of the road that are in good health will be maintained. All tree locations will be coordinated with existing curb cuts and intersections. The south side of the road currently has large mature trees set back from the road that are part of the corporate park; these trees are to remain in place.

Refer to Figure V.3.a - Kaiser Road Existing Conditions, Figure V.3.b - Kaiser Road Proposed Conditions, and Tentative Map: Grading Plans, Utility Plan, Details - Sheets 9 and 15.

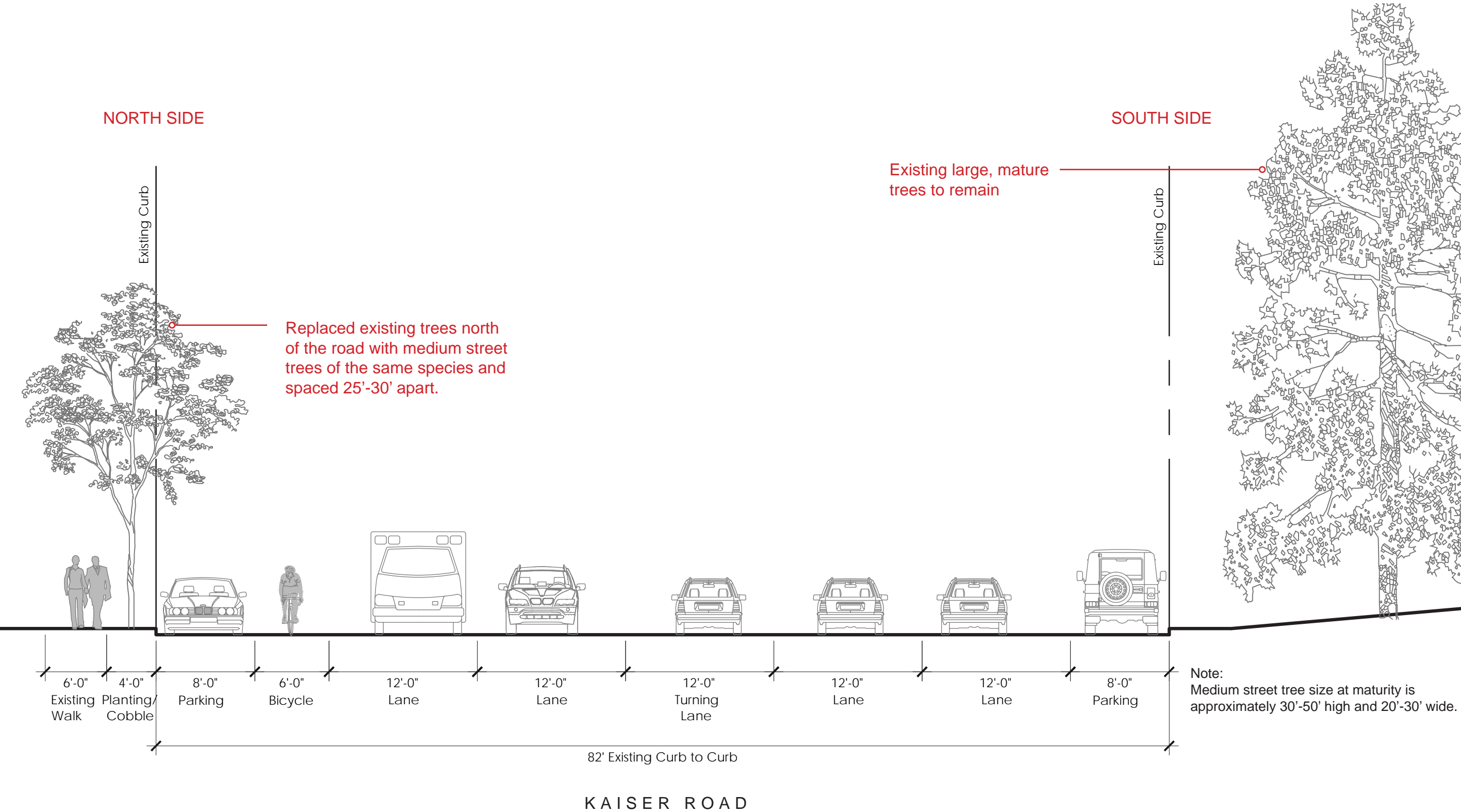
V.3 KAISER ROAD IMPROVEMENTS

Figure V.3.a KAISER ROAD EXISTING CONDITIONS



V.3 KAISER ROAD IMPROVEMENTS

Figure V.3.b - KAISER ROAD PROPOSED CONDITIONS





V.3 KAISER ROAD IMPROVEMENTS

Figure V.3.c - KAISER ROAD PLAN

LEGEND

- Existing face of curb
- Medium street tree, spaced 25'-30' apart

Note:  
Medium street tree size at maturity is approximately 30'-50' high and 20'-30' wide.

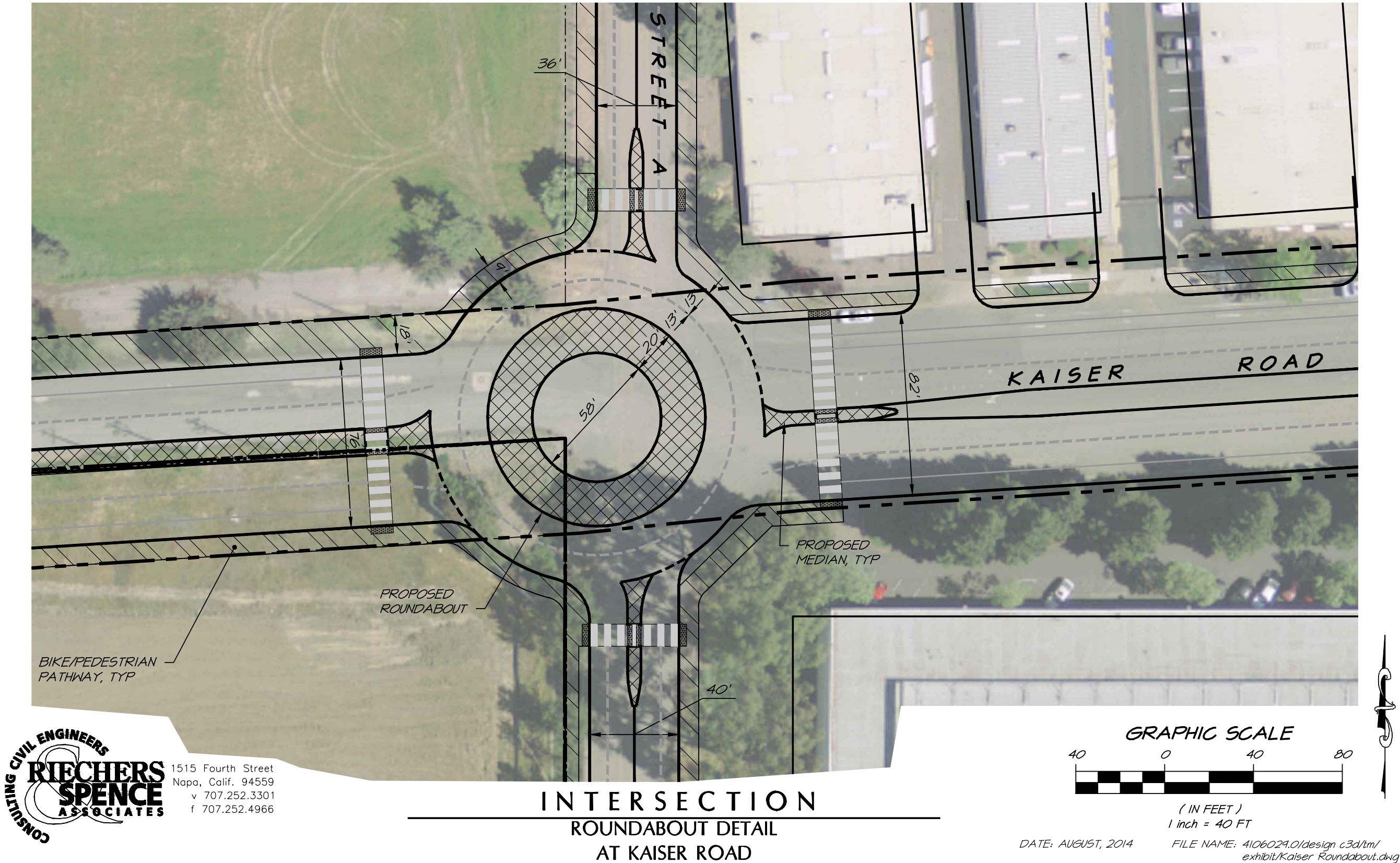


NUMBER OF NEW TREES: 40



V.3 KAISER ROAD IMPROVEMENTS

Figure V.3.d - KAISER ROAD TRAFFIC CIRCLE

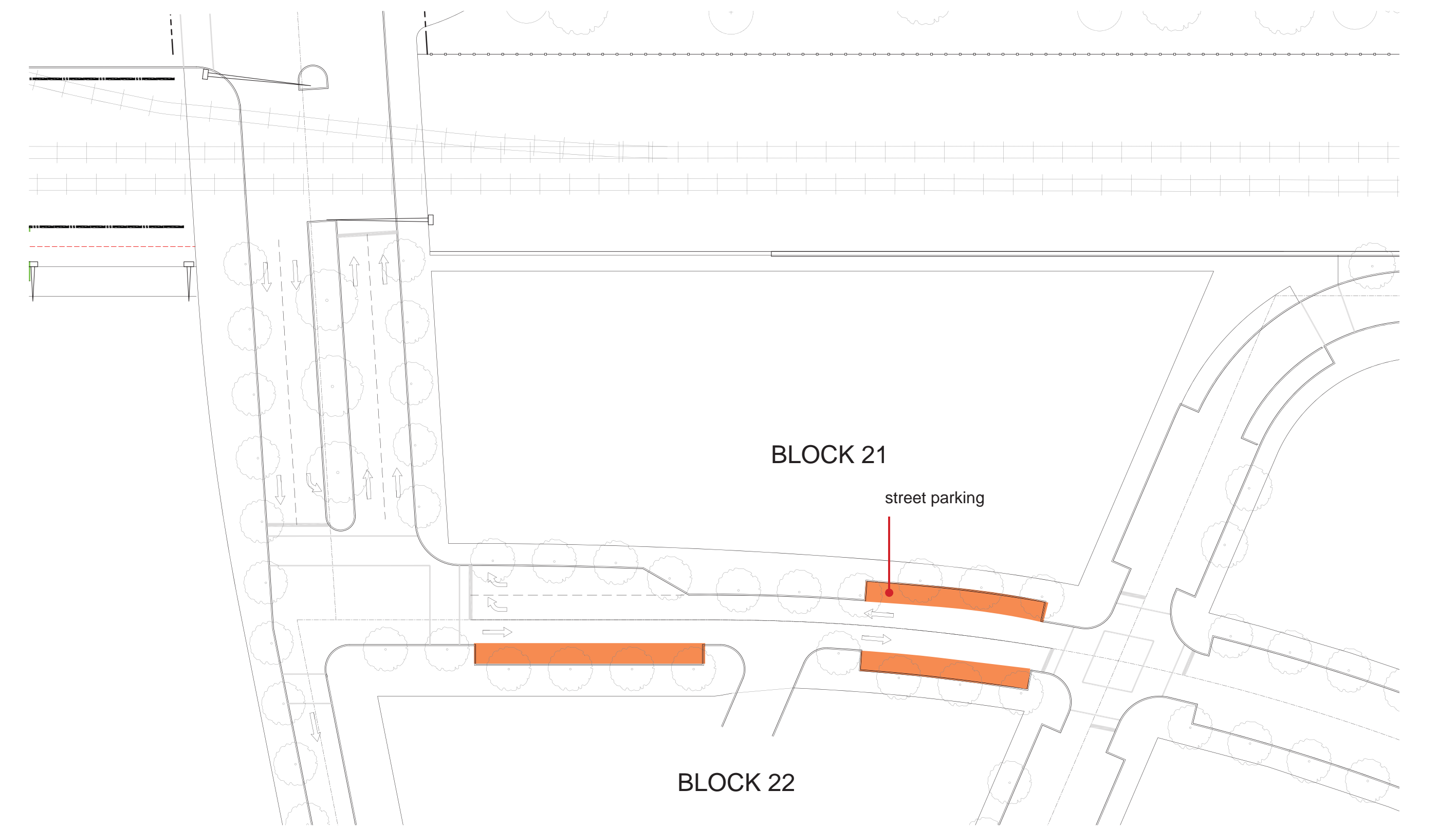


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**RIECHERS**  
**SPENCE**  
**ASSOCIATES**

1515 Fourth Street  
Napa, Calif. 94559  
v 707.252.3301  
f 707.252.4966

V.3 KAISER ROAD IMPROVEMENTS

Figure V.3.e TRANSITION FROM KAISER ROAD TO NEIGHBORHOOD ROADS





V.4 OPENING DAY MITIGATION

Fehr and Peers analyzed the opening day traffic demands for the project and provided analysis of intersections leading to the site under peak demand scenarios.

The study identified intersections that require improvements prior to occupancy of the proposed project. These four intersections are as follows:

- Imola Avenue / Soscol Avenue
- Highway 221 / Streblow Drive
- Napa Valley Corporate Drive / Anselmo Court
- Soscol Ferry Road / Devlin Road

Further explanation of the required improvements can be found in Exhibit J of Volume 2 of the development plan.

The following is an Excerpt from Fehr & Peers Report Included as Appendix E of the Draft Environmental Impact Report, Fehr & Peers Memo, Supplemental Environmental Assessment, September 2012.

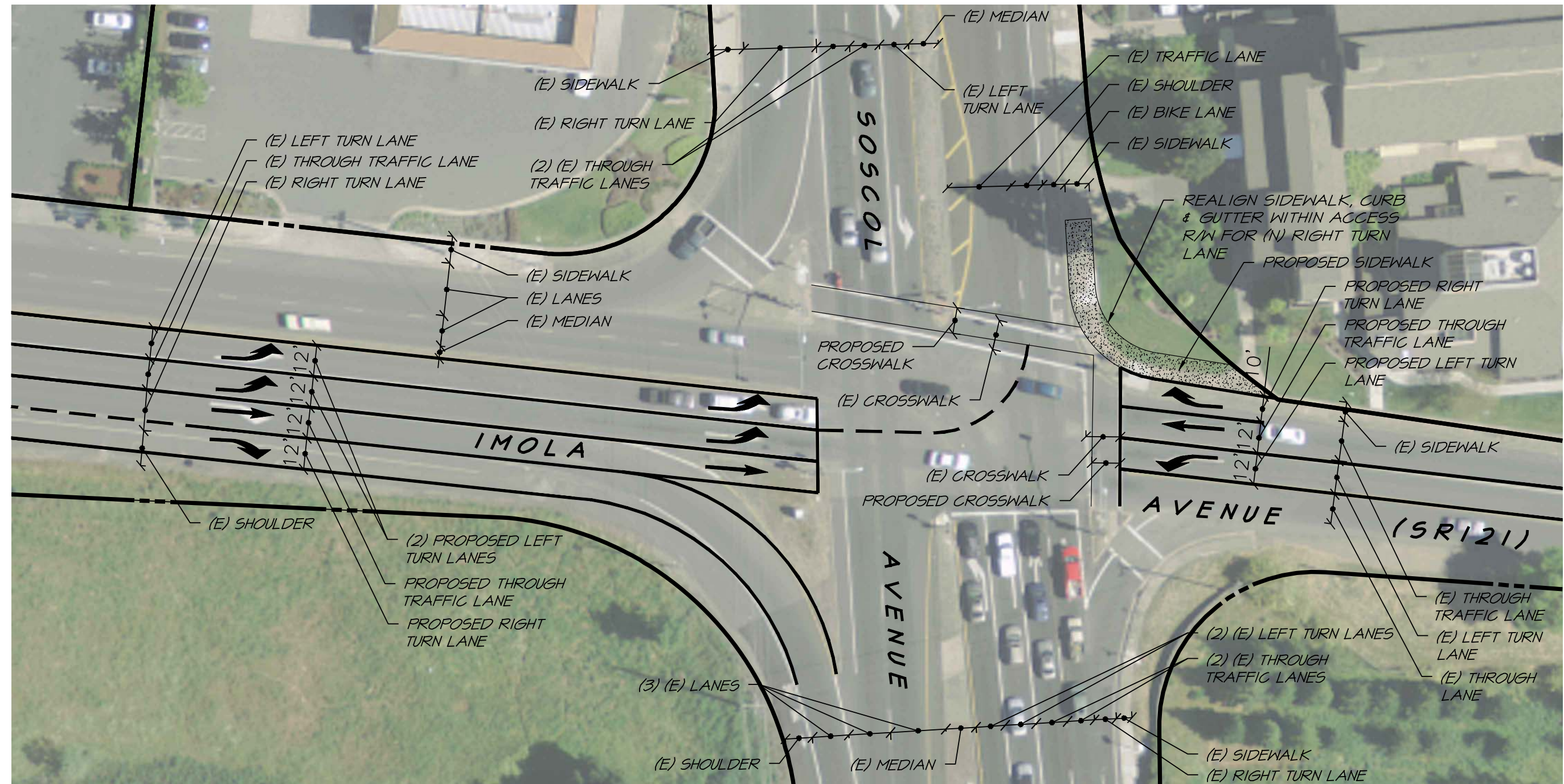
“The Solano-Napa County travel demand model was used to develop forecasts at study intersections. At some locations, particularly in the City of Napa, forecasts had been previously developed as part of other studies. A comparison of forecasts developed using the County TDF model with forecasts developed as part of other studies revealed that the previously-developed forecasts within the City of Napa were higher. Therefore, to be conservative, these higher forecasts were used where available.

“Traffic volumes in Napa County vary over time due to seasonal employment and tourism. Although the volumes used in this analysis were based on typical peak conditions (Thursday in August), a sensitivity test was conducted to ensure that cumulative improvements identified would continue to be sufficient to provide acceptable operations. Traffic forecasts were increased by 10 percent and intersections were re-analyzed. (Intersections in the City of Napa where forecasts from previously-completed studies were used were not subject to this sensitivity testing because they were already conservatively higher than predicted by the county TDF model).”

Refer to Figure V.4.a - Opening Day Mitigation Measures: Intersections 12, 13, 22, & 25; Figure V.4.b - Conceptual Signal Design: Napa Valley Corporate Drive / Anselmo Court; and Exhibit I - Kittelson Memo, in Vol. 2

## V.4 OPENING DAY MITIGATION

Figure V.4.a - OPENING DAY MITIGATION MEASURES - INTERSECTION #12



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v 707.252.3301  
f 707.252.4966

INTERSECTION # 12  
SOSCOL AVENUE / IMOLA AVENUE (SR 121)  
MITIGATION # TRA-5

### GRAPHIC SCALE



( IN FEET )  
1 inch = 40 FT

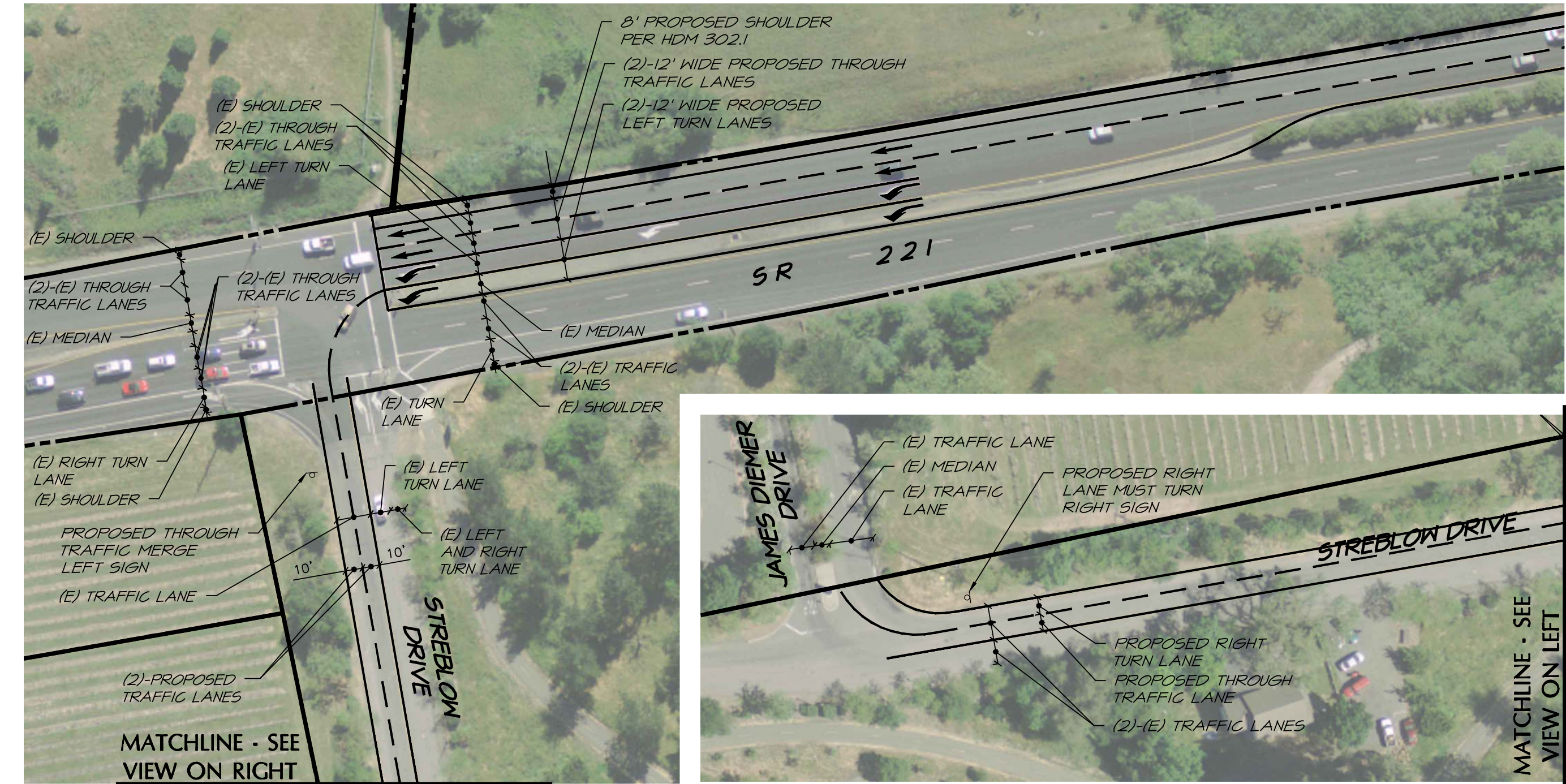
DATE: April 9, 2014

FILE NAME: 4106029.0/design c3d/exhbt/  
opening day MM #12



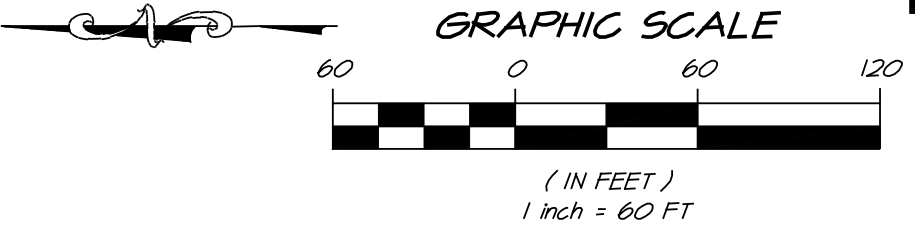
V.4 OPENING DAY MITIGATION

Figure V.4.b - OPENING DAY MITIGATION MEASURES - INTERSECTION #13



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Napa, Calif. 94559  
v 707.252.3301  
f 707.252.4966

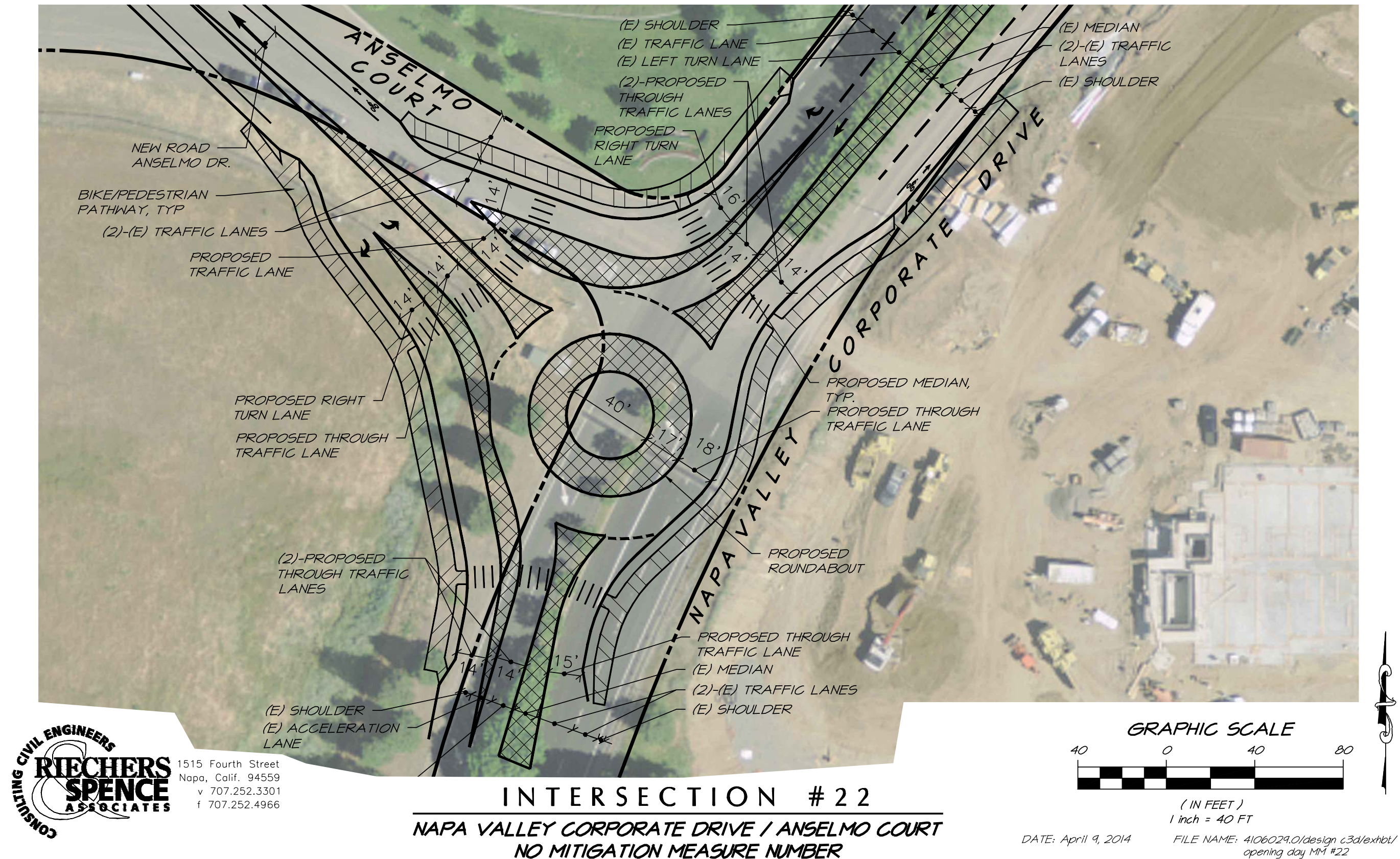
**INTERSECTION #13**  
**SR 221 / STREBLOW DRIVE**  
**MITIGATION MEASURE #TRA-6**



DATE: April 9, 2014  
FILE NAME: 4106029.0/design c3d/exhbt/opening day MM #13



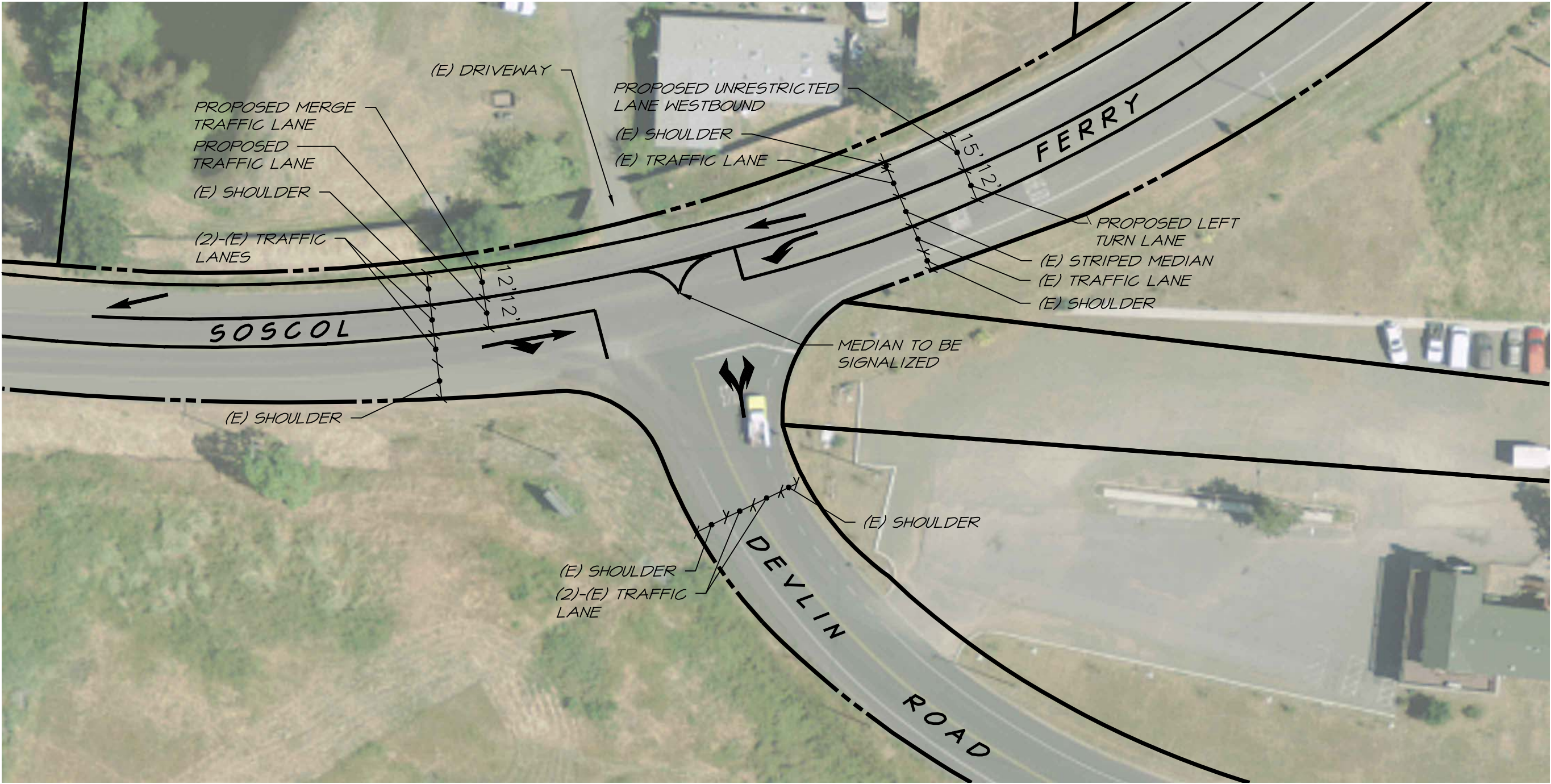
Figure V.4.c - OPENING DAY MITIGATION MEASURES - INTERSECTION #22





V.4 OPENING DAY MITIGATION

Figure V.4.d - OPENING DAY MITIGATION MEASURES - INTERSECTION #25



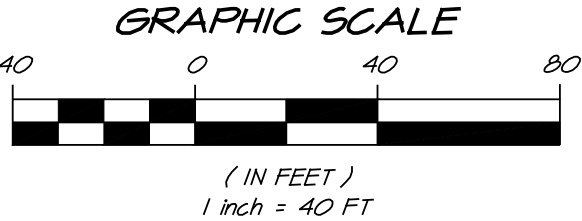
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**RIECHERS  
& SPENCE**

ASSOCIATES

1515 Fourth Street  
Napa, Calif. 94559  
v 707.252.3301  
f 707.252.4966

INTERSECTION #25  
SOSCOLD FERRY ROAD / DEVLIN ROAD  
MITIGATION MEASURE #TRA-8

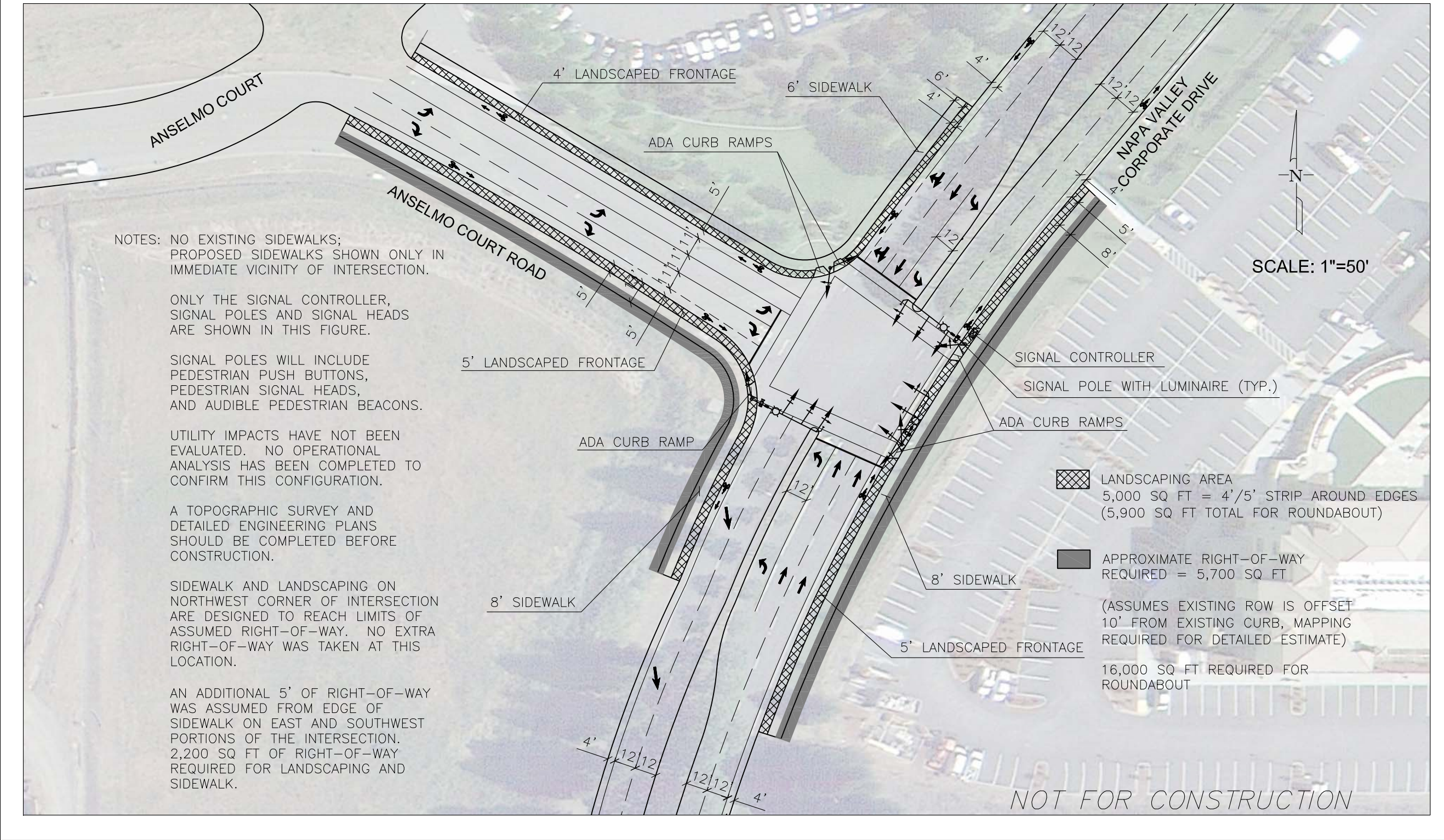


DATE: April 9, 2014      FILE NAME: 4106029.0/design c3d/exhbt/ opening day MM #25



V.4 OPENING DAY MITIGATION

Figure V.4.e CONCEPTUAL SIGNAL DESIGN: NAPA VALLEY CORPORATE DRIVE / ANSELMO COURT





V.5 ON & OFF-STREET PARKING STANDARDS

Table V.5.a - Proposed Parking Standards

			Napa County NP zoning		
Western Parcel			w/out guest		
Residential Parking (For Sale Units)	% of Total	Units/SF	Spaces per Unit	Total Spaces	
Studios	5%	38	1.25	48	48
1-Bedroom	25%	189	1.50	284	284
2-Bedroom	45%	340 units	2.00	680	680
3-Bedroom	25%	189	2.00	378	378
Guest Parking		756 units	0.25	189	
Residential Parking (Rental Units)	% of Total				
Studios	5%	9 units	1.25	11	11
1-Bedroom	25%	47 units	1.50	71	71
2-Bedroom	45%	85 units	2.00	170	170
3-Bedroom	25%	47	2.00	94	94
Guest Parking		189 units	0.25	47	
Senior Housing					
Units		150 units	1.00	150	150
Guest Parking		150 units	0.25	38	
TOTAL RESIDENTIAL PARKING		1095 units		2,160	1,886
Average (including Guest parking)				1.97	1.72
Non-Residential Parking					
Restaurants (Full Service)		15,000 sf	1 space per 120 sf (included)	125	
Employees		15,000 sf			
Retail (Neighborhood Shopping)		25,000 sf	1 space per 250 sf (included)	100	
Employees		25,000 sf			
Condominium Hotel					
Rooms		150	1 per room (included)	150	
Employees		80			
Meeting Area		20,000 sf	20/1,000 sf	250 <sup>2</sup>	
Office (Including Employees)		10,000 sf	1/250 sf	40	
TOTAL NON-RESIDENTIAL PARKING				665	
TOTAL WESTERN PARCEL				2,825	
Eastern Parcel					
Non-Residential Parking Western Parcel					
Office (Including Employees)		90,000 sf	1/250 sf	360	
R&D / Light Industrial / Warehouse (Including Employees)		75,000 sf	1/500 sf 1/1,000 for first 20k sf, 1/2,000 greater than 20,000 sf	132	
General Wholesale Commercial		154,000 sf	1/200 sf	770	
TOTAL EASTERN PARCEL				1,262	
TOTAL PARKING				4,072	

<sup>1</sup> 96% of peak demand per Walker Parking Study  
<sup>2</sup> Assumes a maximum of 500 people, per Napa Redevelopment Partners

The project aims to reduce the areas of paved surfaces, reduce trip generation and GHG emissions and reduce dependency on fossil fuels. Through careful analysis of relevant standards by Walker Parking Consultants, the project proposes to modify the parking standards of the County. This proposed standard recognizes the import of personal vehicles to the site and environs from residents, patrons and workers alike. Please see accompanying table of proposed standards: Table V.5.a - Proposed Parking Standards

The parking standards set forth are intended to promote the pedestrian and street life of the project. In order to activate the street with pedestrian activity of people walking to and from their parked cars the project utilizes the available street parking first for guest and patron parking, and the remaining for residential parking.

**Residential**  
In residential areas, only one off-street stall per unit would be required to be assigned parking. The remaining required parking spaces could be shared and unassigned. Residential parking would be provided in a mix of off street, on-street, and on street guest parking. Each block would be required to provide the calculated guest parking demand on the streets directly adjacent to the respective block. The remaining on-street spaces would be available for un-allocated shared residential parking.

This high number of shared parking spaces, provides a considerable buffer to accommodate uneven distribution of demand. The worst demand period in residential areas are weekend evenings, when residents are generally home and entertaining guests (though some residents are also away to visit friends elsewhere).

Similar to retail or commercial, each parcel could count towards its parking supply the on-street parking stalls directly adjacent to the parcel. A parking study based on the ITE manual would determine the actual demand for each parcel. Parking for patrons would be on the street, with any unmet balance provided on site. Those spaces would be marked as guest parking. If the number of required patron spaces is less than the number of available on-street spaces, the balance may be used for employee parking.

**Mixed Use**  
The mixed use area around the drydocks would have parking for business patrons and residential guests provided on street; employee parking would be provided off-street. The projected peak parking demand for the neighborhood-serving businesses is 195 stalls (occurring on a weekend in December at 8:00 PM), which would be provided on street within a half-mile walking distance from the drydock area.

At completion of each phase, a parking survey will be conducted to assess the existing parking demands and trends, per TRA-18 from Exhibit XI, MMRP:

**“TRA-18:** To address issues associated with off-street parking supply, the project applicant shall collaborate with County Staff to develop a parking monitoring plan that assesses the utilization of available parking, to be included in the development plan. Alternatively, implementation of a parking management program, a component presented in Mitigation Measure TRA-1b, could be implemented to monitor parking demand and carry out parking reduction strategies when needed.”

Refer to Figure V.5.a - Parking Test Fit

Based on the proposed standards, the parking allocations for the project are anticipated to be as follows in the accompanying table V.5.b. - Parking Allocations

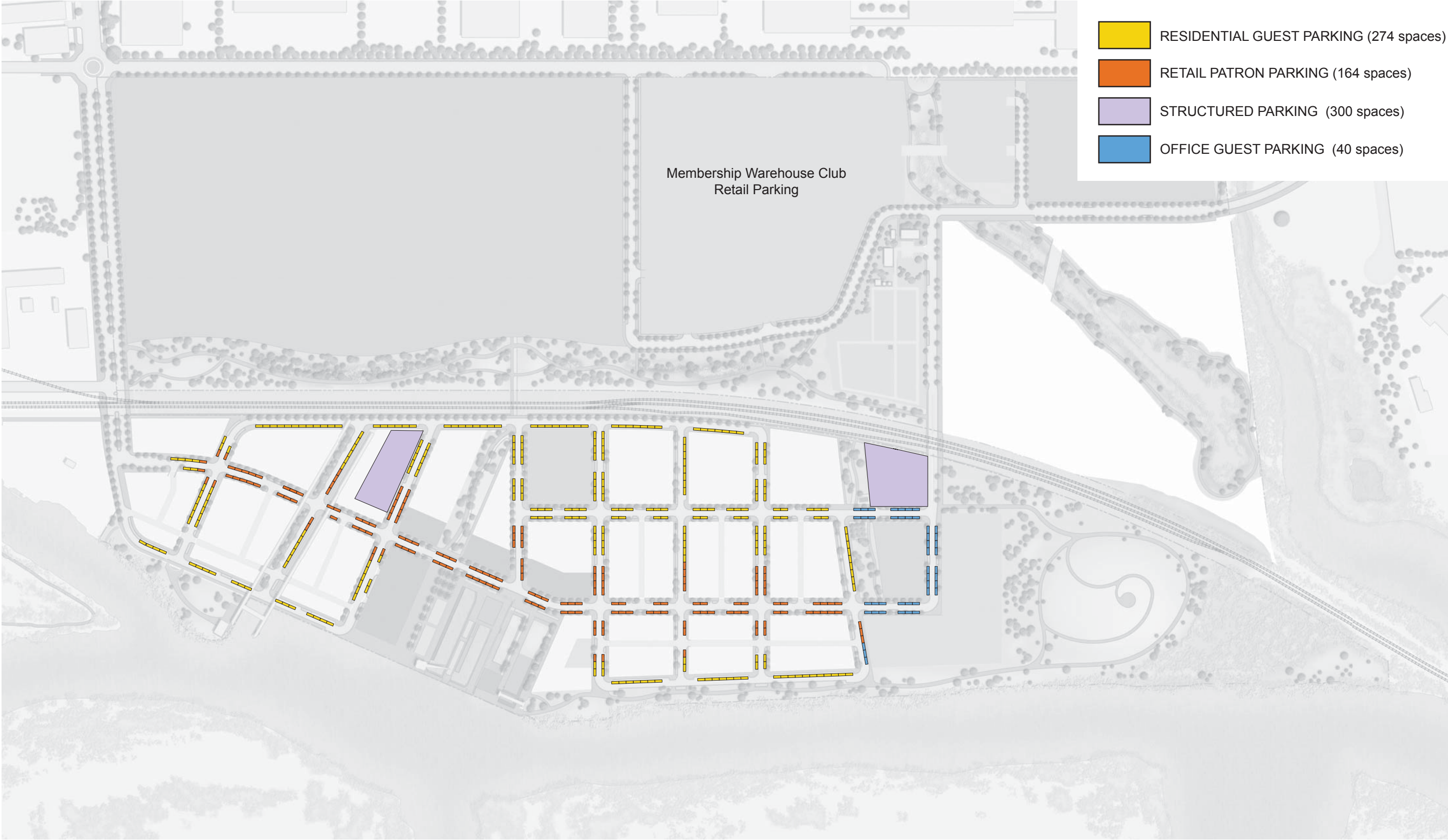
Table V.5.b - Parking Allocations

	Required	Provided
TOTAL PARKING REQUIRED (Western Parcel)		
Retail Parking Required	225	
On-Street Parking within 1/4 mile radius		164
Retail Parking Accommodated Off Street		61
Structured Parking (only if required by parking survey, per TRA-18, Exhibit XI, MMRP)		
Hotel Parking Required	400	
Office Parking Required	40	
Structured Parking		300
Commercial Parking Accommodated On Street		45
Parking Required Off Street		80
Residential Parking Required	2,160	
Resident Parking Required		1,886
Guest Parking Required		274
		2,825



V.5 ON & OFF-STREET PARKING STANDARDS

Figure V.5.a PARKING TEST FIT





VI.1 WETLAND RESTORATION PLAN

Figure VI.1.a STORMWATER MANAGEMENT AND WETLANDS OVERVIEW



*Excerpt from Zander and Associates Habitat Preservation and Open Space Elements Report*

Mitigation measures are to be implemented to lessen the impact of development on the existing wetlands on the project site. This includes the establishment of a 10 foot wide 'no-touch zone', a 25 foot construction setback for pathways and a 50 foot setback for other improvements. Steps will be taken to remove non-native plants within the wetland area and any encountered hardscape or other features with the 10 foot 'no-touch zone' will be removed and grade restored and replanted with approved native species.

Refer to Figure VI.1.a - Stormwater Management and Wetlands Overview, Figure VI.1.b - Bioretention Study, and Exhibit aO.1 - Excerpts from "Habitat Preservation and Open Space Elements Report," May 2009.

LEGEND

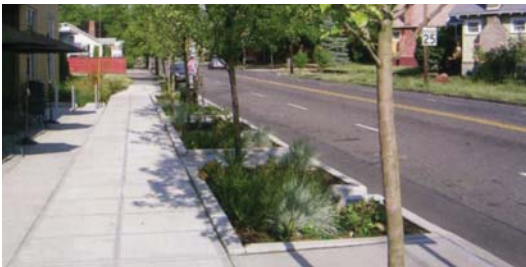
- ..... Wetland Buffer
- Constructed wetlands
- Existing wetlands
- Biofiltration areas



EXISTING WETLANDS



BIOFILTRATION PRECEDENT



BIOFILTRATION PRECEDENT

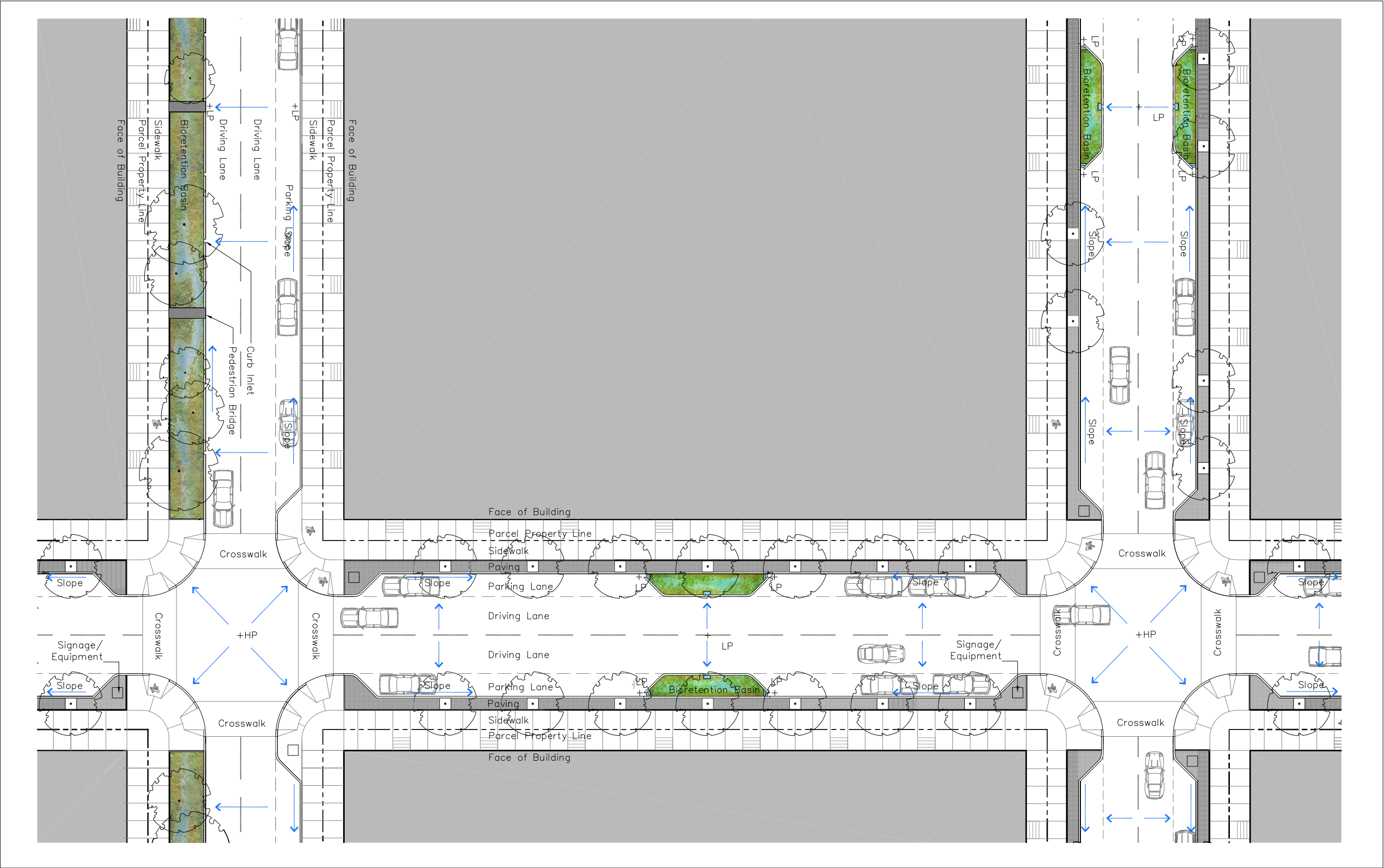


BIOFILTRATION PRECEDENT



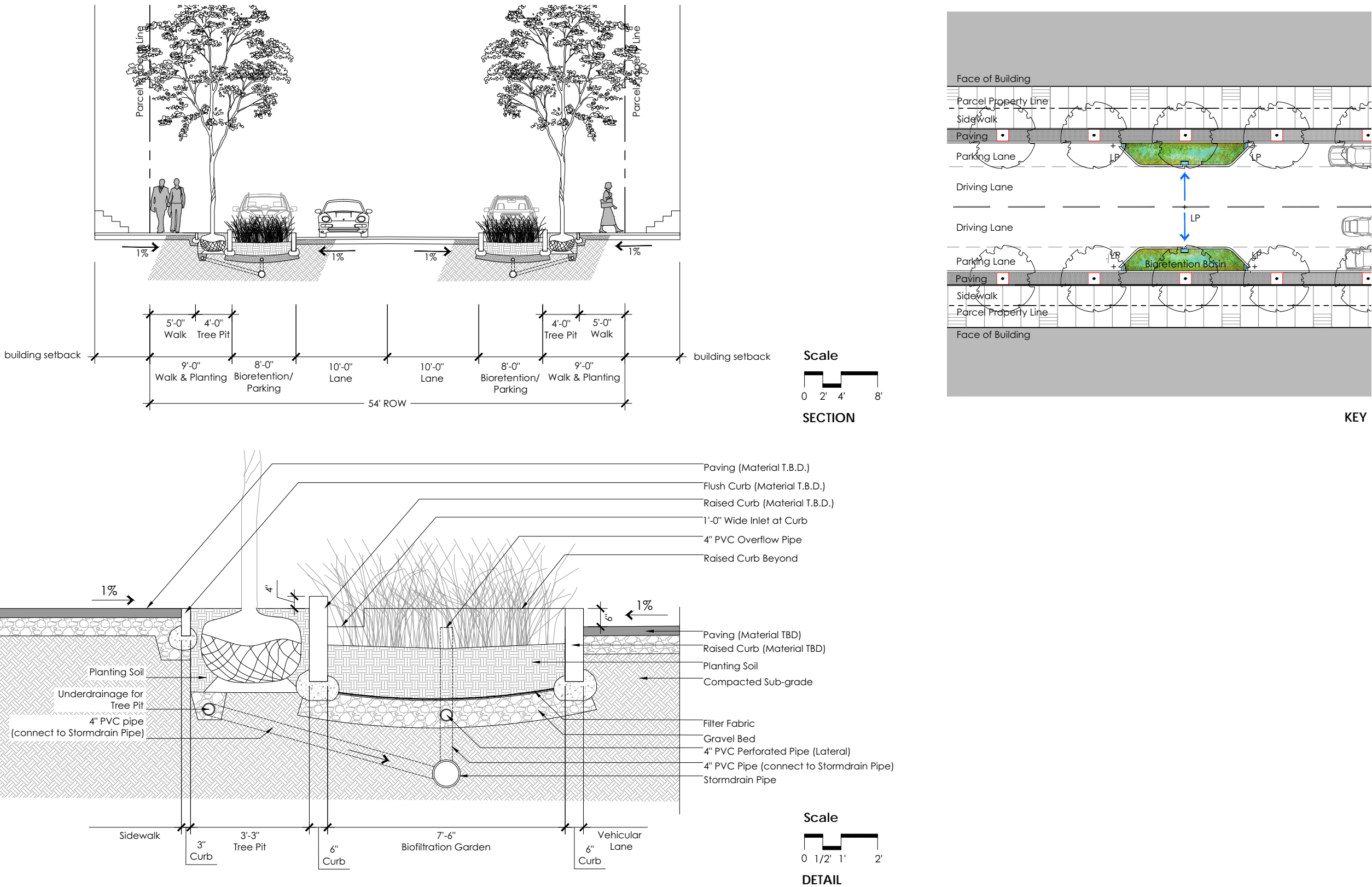
VI.1 WETLAND RESTORATION PLAN

Figure VI.1.b BIORETENTION STUDY



VI.1 WETLAND RESTORATION PLAN

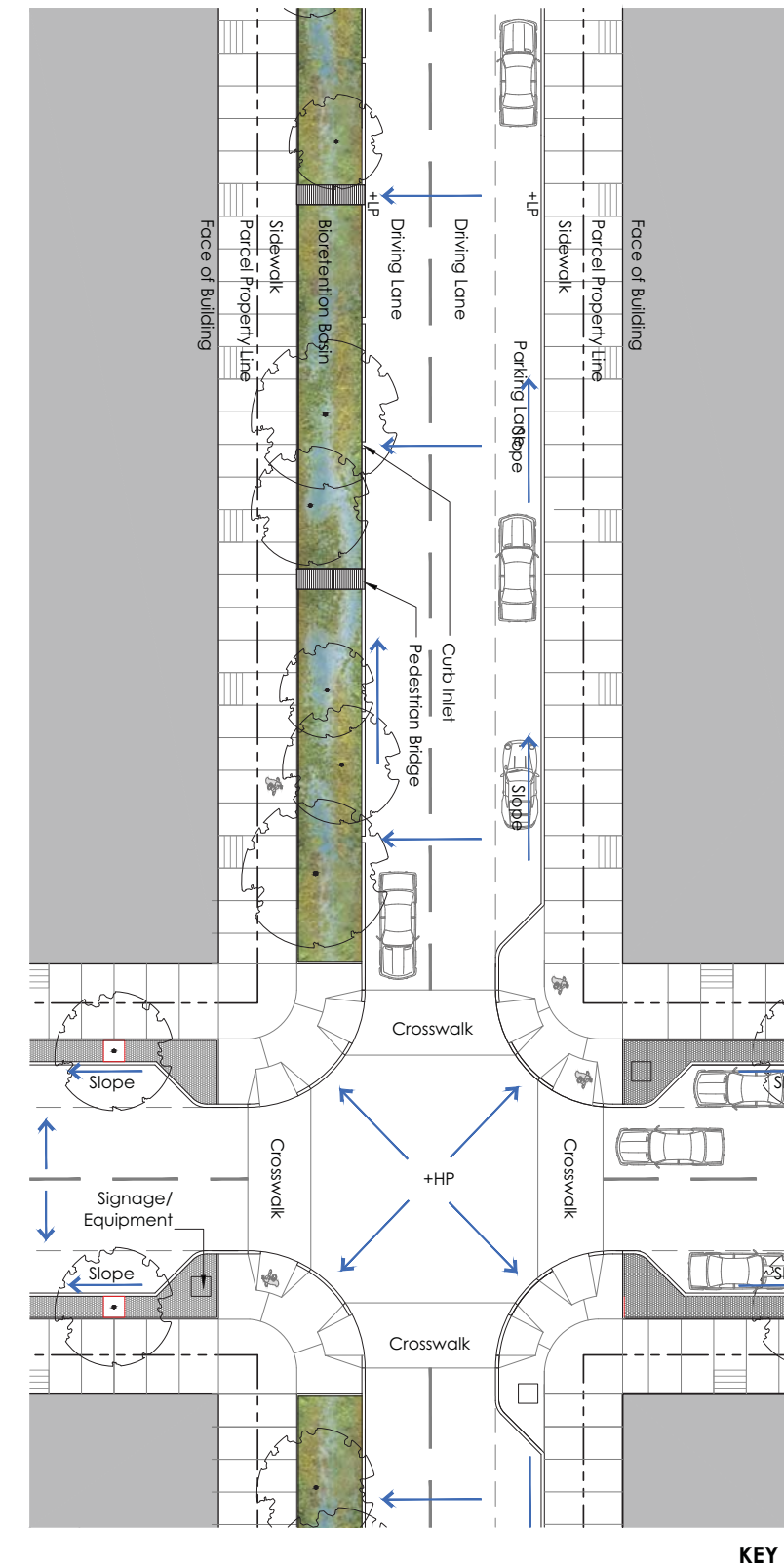
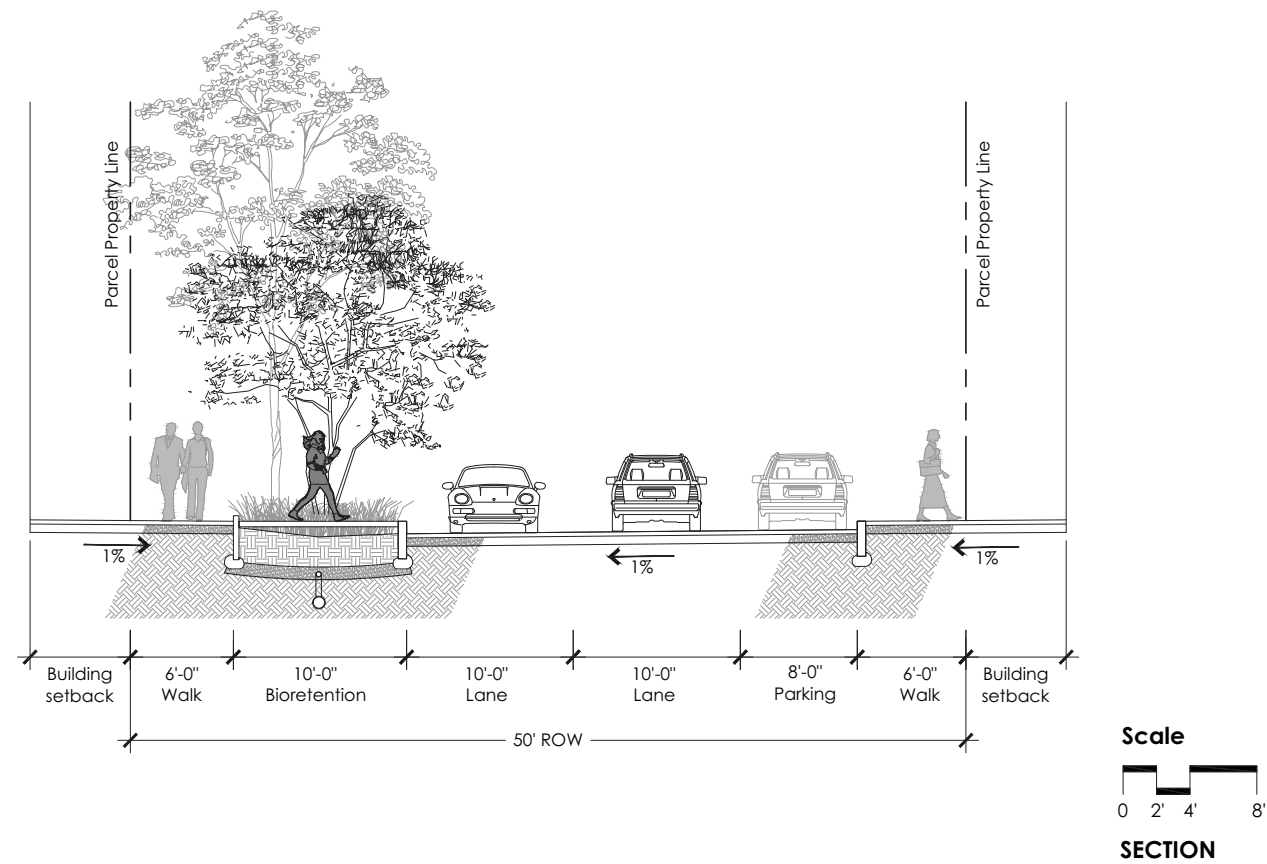
Figure VI.1.c BIORETENTION STUDY





## VI.1 WETLAND RESTORATION PLAN

Figure VI.1.d BIORETENTION STUDY



VI.2 PRESERVATION & INTERPRETATION OF HISTORIC RESOURCES

Figure VI.2.a - PRESERVATION & INTERPRETATION OF HISTORIC RESOURCES: Overview



Public Benefits – Industrial Artifacts

Several industrial artifacts have been retained and incorporated into the site plan to remind visitors of the site's history and to provide visual interest within the development. The salvaged industrial artifacts include the following:

- Existing Warehouse Structure
- Bridge Crane
- Seawall
- Seawall Crane
- Gantry Crane
- Drydocks

All industrial artifacts will remain in their existing location. The engines and mechanical components of each crane will be decommissioned to insure all fluids such as engine fuel or oil is properly removed and disposed of, and to insure all moving parts are anchored in place. Where needed, anti-climbing hardware will be installed to prevent climbing on or inhabiting the structures. All metal will be cleaned and sealed or painted (color to be determined). All non-essential pieces will be stripped from the structures and properly recycled.

Mitigation Measure CULT-1b requires than an interpretive display featuring the shipyard's history be incorporated into the project. This display shall be located in an area accessible to the public and shall provide information regarding the historical contributions of the Basalt Shipyard. The display will help to place the dry docks in context for the public. (See *Exhibit B to the Board of Supervisors June 4, 2013 Resolution No. 2013-60 - Mitigation Monitoring & Reporting Program*, attached as Exhibit R in Vol. 2)

This display component has tentatively been considered at the river's edge adjacent to the most visible historical items proposed to be retained in the project. At each of the historic structures a sign would explain their historic context and relation to each other and in so doing create a historic walk within the project.

Refer to Figures VI.2.a - VI.2.f, and Vol. 2 - Exhibit R - Mitigation Monitoring and Reporting Program



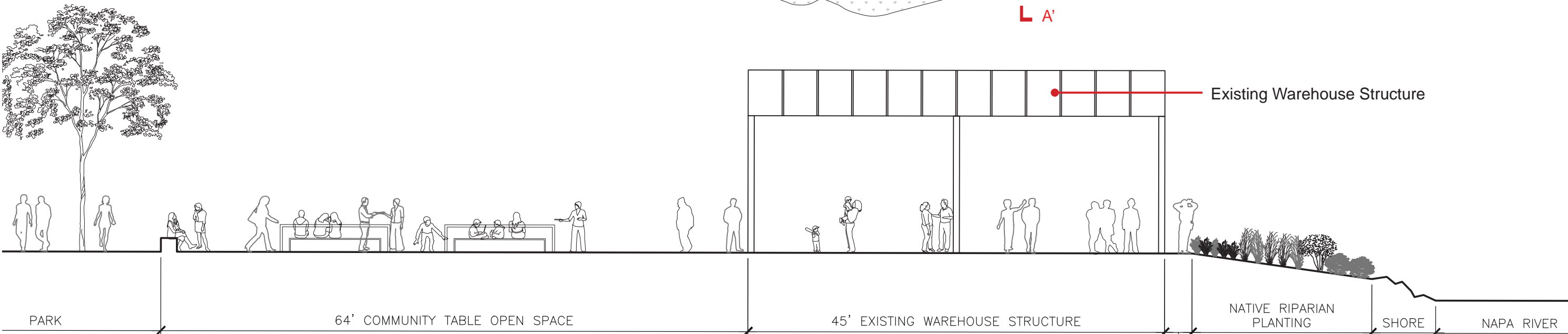
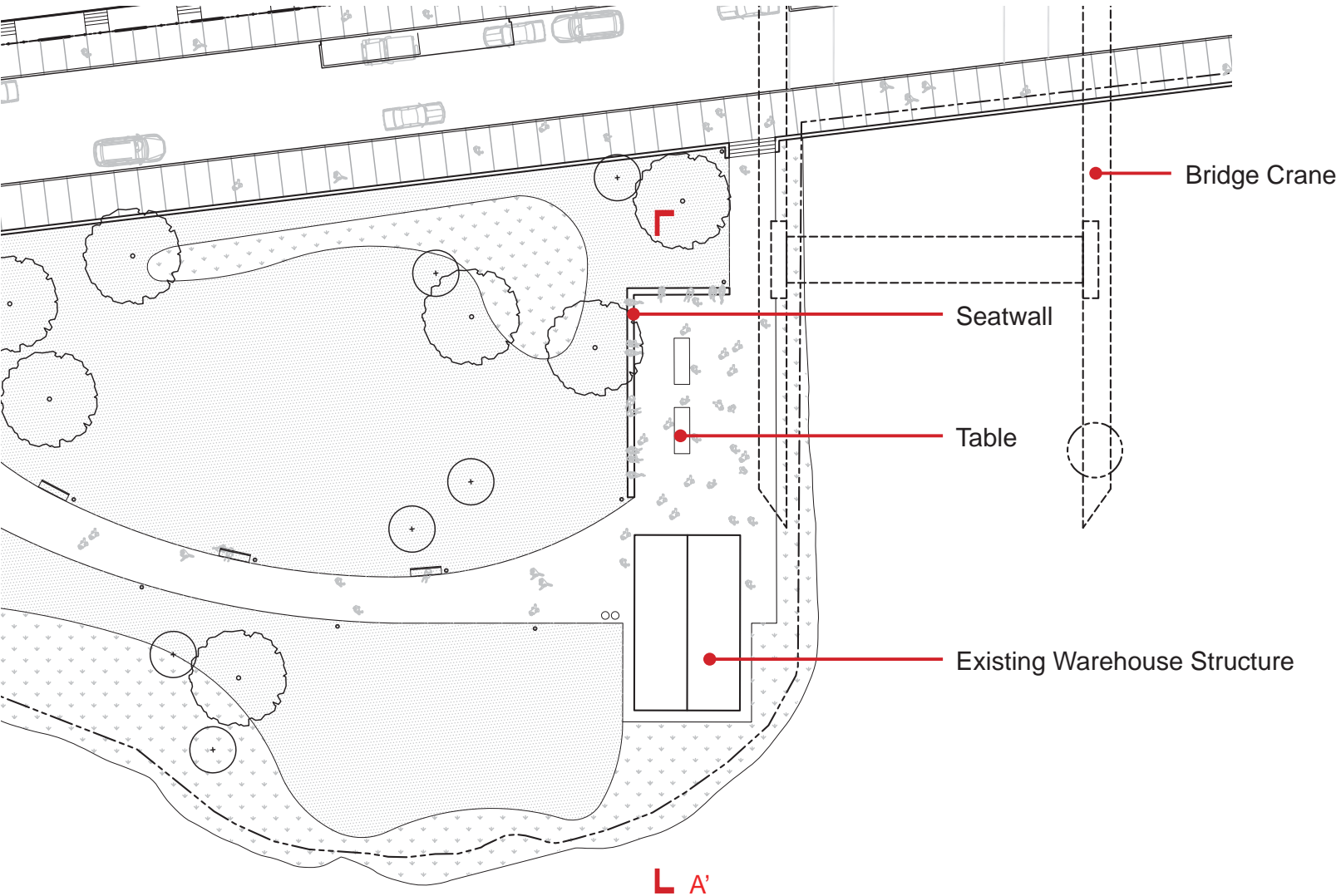
VI.2 PRESERVATION & INTERPRETATION OF HISTORIC RESOURCES

Figure VI.2.b - EXISTING WAREHOUSE STRUCTURE



Existing Warehouse Structure

The retrofitted structure will serve as an open air shelter for community gatherings and picnics in the Community Park (P6). The skin of the existing structure will be removed, the structural members will be left exposed, and a new roof will be installed. The interior of the open air structure will be retrofitted with lighting for night-time use, power outlets, as well as multipurpose tables and chairs.



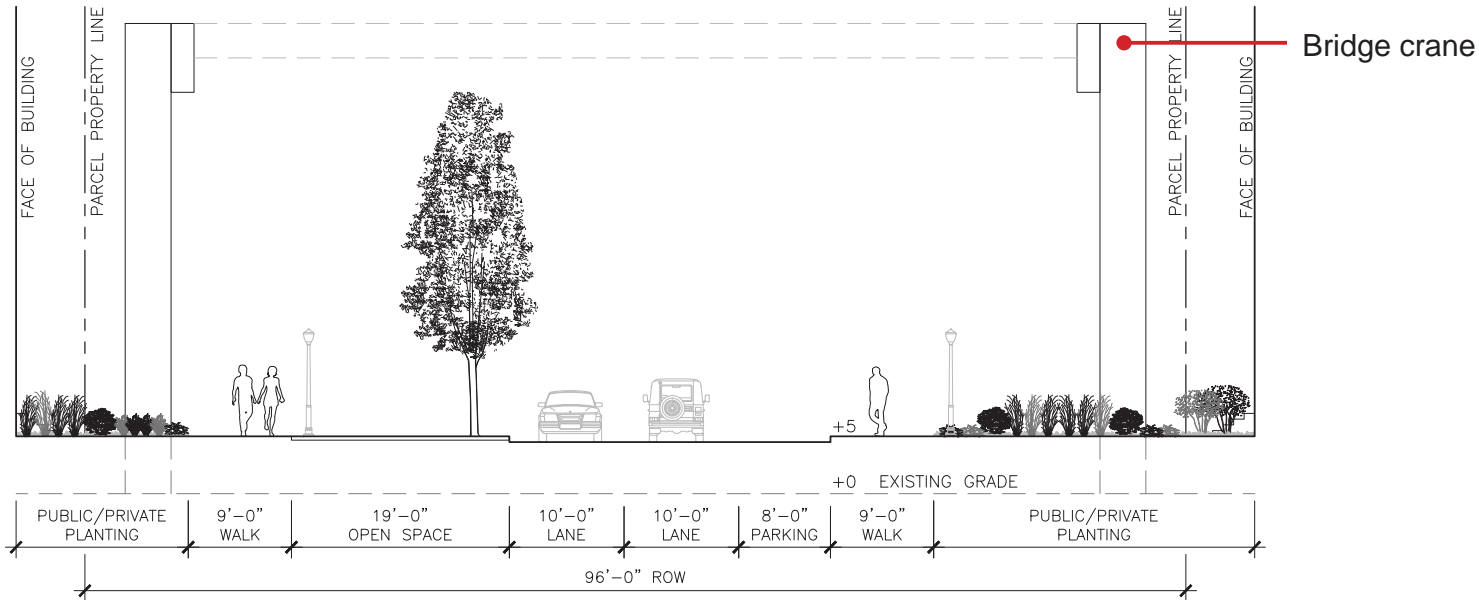
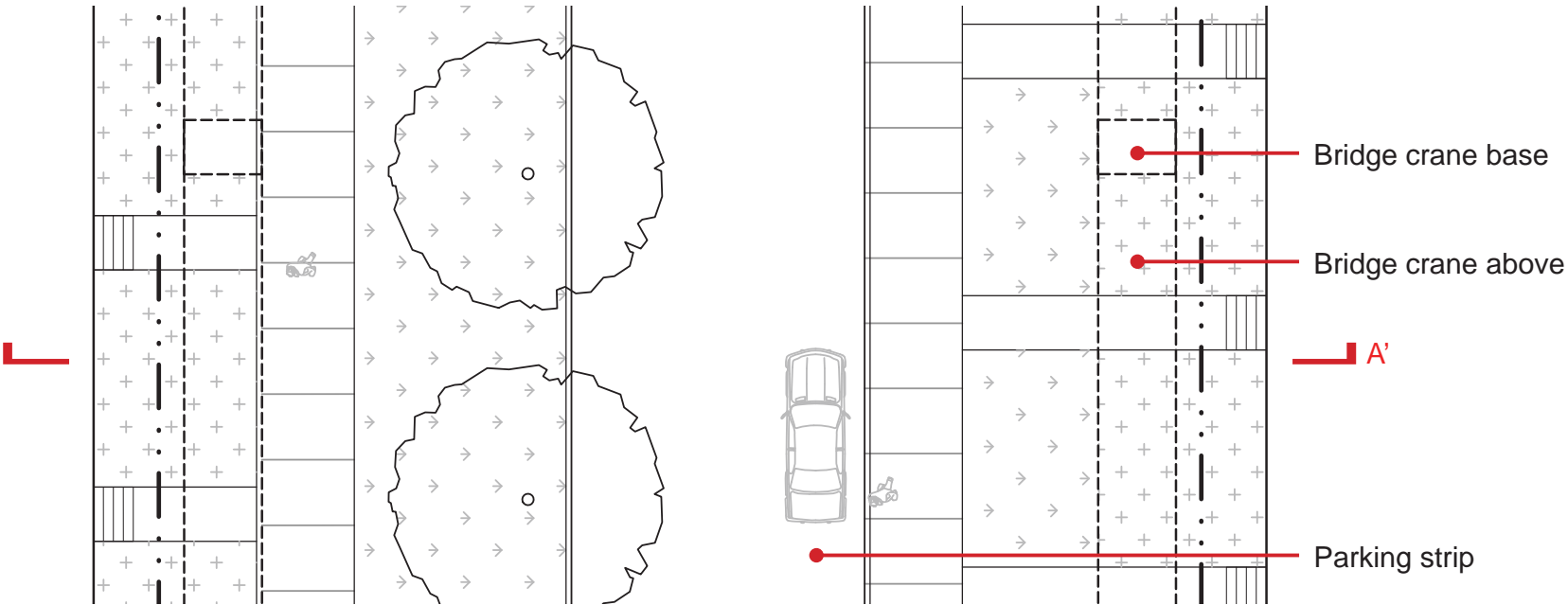
VI.2 PRESERVATION & INTERPRETATION OF HISTORIC RESOURCES

Figure VI.2.c - BRIDGE CRANE



Bridge Crane

The large columns supporting the horizontal rails will be in the public ROW and will sit in either planting or paving. The horizontal rails will span over the residential streets and will not be physically connected to the adjacent residential structures. The gantry crane ‘cab’ will be mechanically anchored to the horizontal rails in one fixed location near the western edge of the bridge structure. The bridge structure will be a strong visual anchor for the street and will be an iconic landmark in the neighborhood. Horizontal and vertical rails may serve as support structures for street lighting and accent lighting and additional uplights with the planting beds may be used to architecturally highlight the structure.





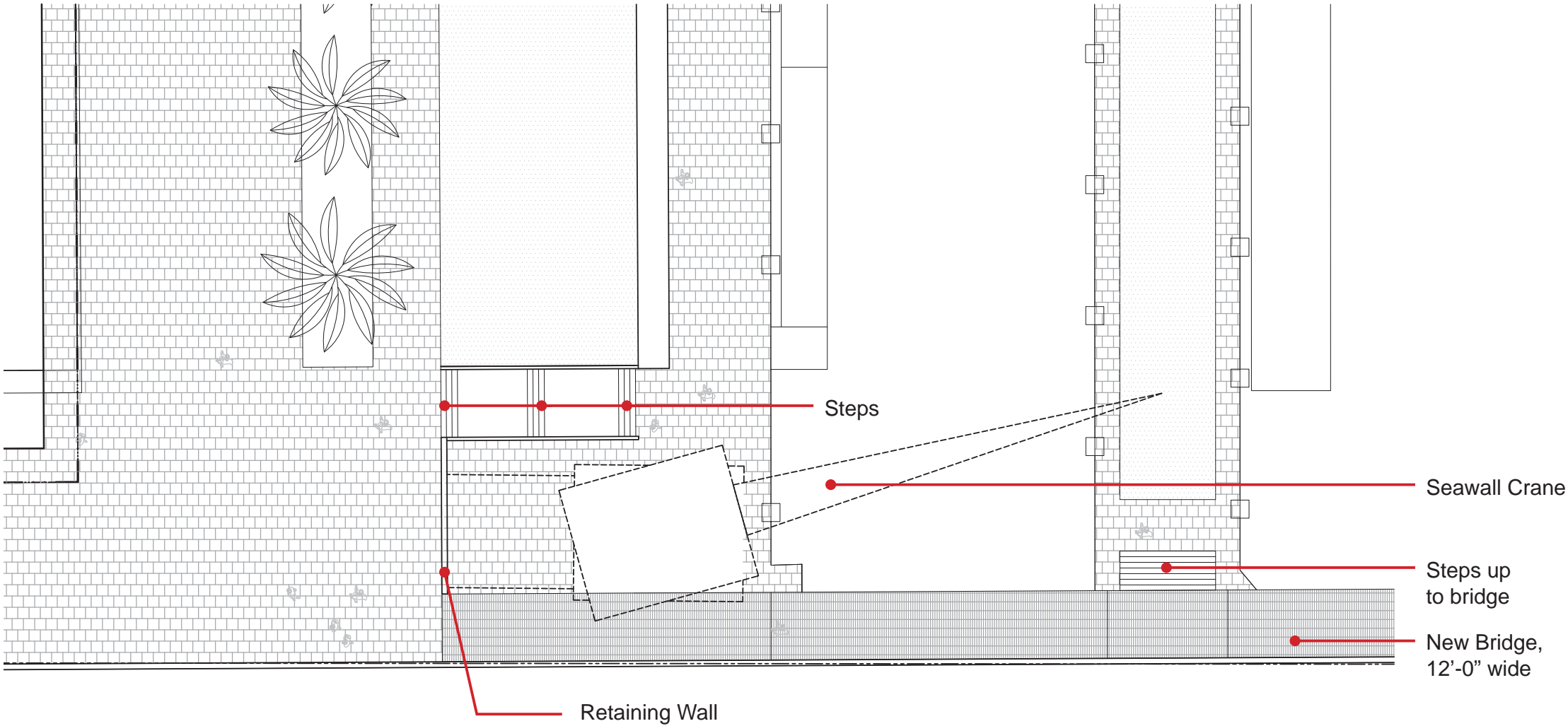
VI.2 PRESERVATION & INTERPRETATION OF HISTORIC RESOURCES

Figure VI.2.d - SEAWALL CRANE



Seawall Crane

The seawall crane will be mechanically anchored to the pavement in a fixed location near the northwest corner of the drydocks. The crane will be an iconic landmark in the drydock plaza. The crane will be lit by a series of paving uprights and or adjacent pole lights.



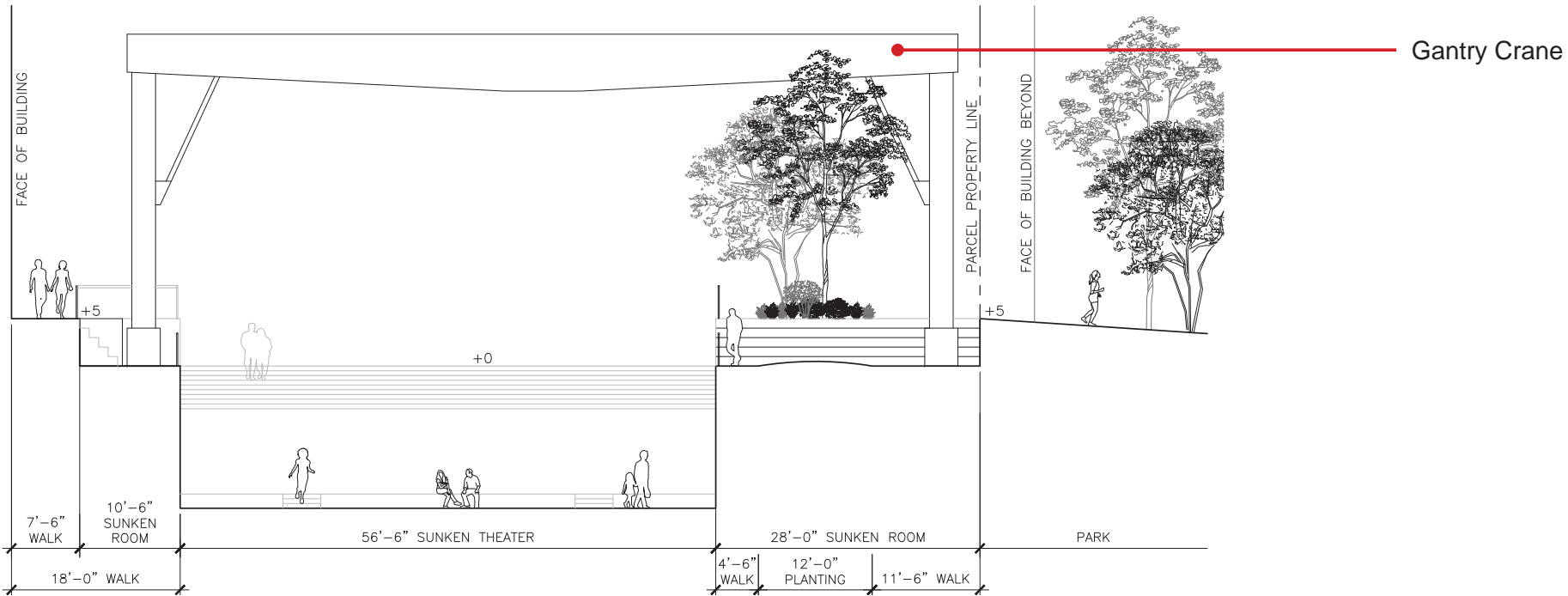
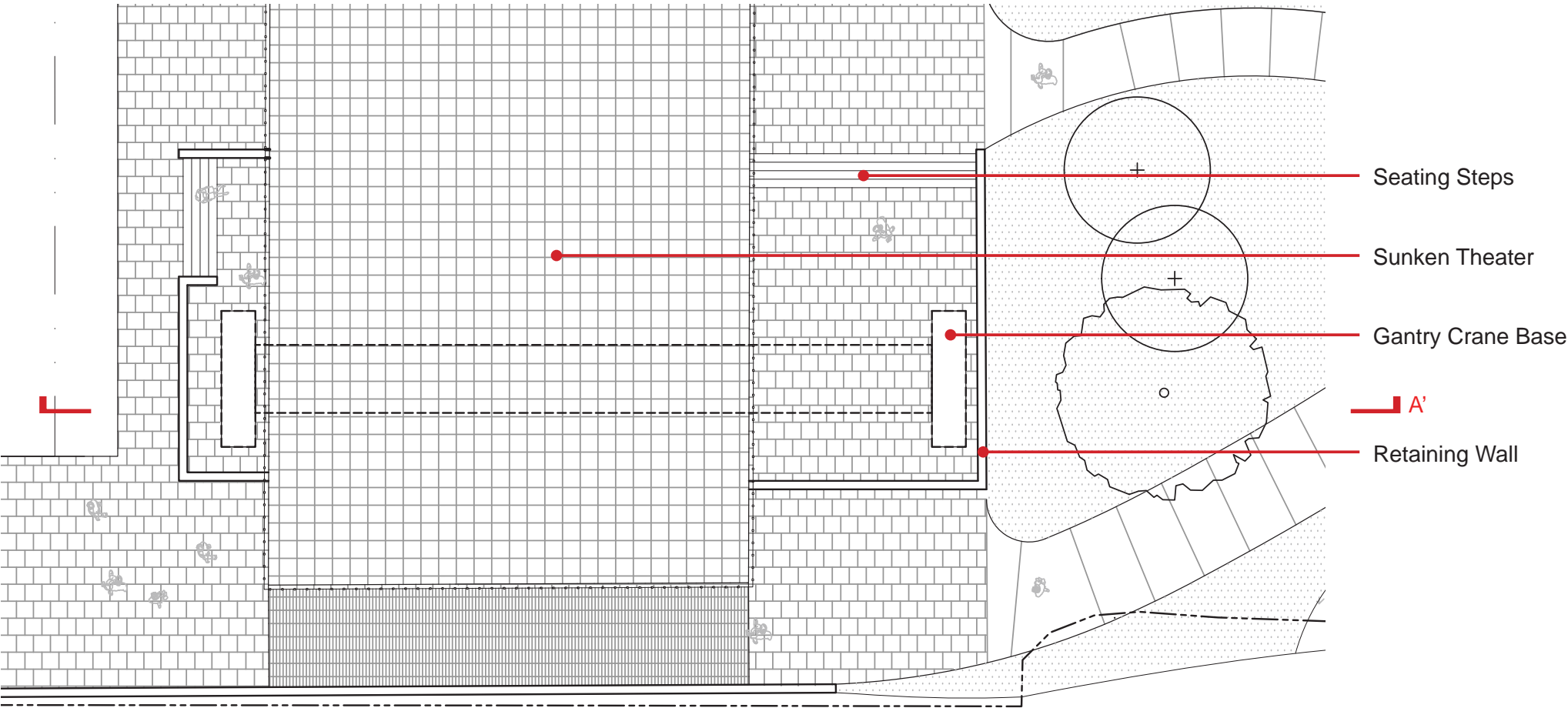
VI.2 PRESERVATION & INTERPRETATION OF HISTORIC RESOURCES

Figure VI.2.e - GANTRY CRANE



Gantry Crane

The gantry crane ‘cab’ will be mechanically anchored to the pavement in a fixed location near the western end of the future drydock theatre. The crane will support an operable outdoor screen and associated sound system that can be used for outdoor projections/ movies. The crane will be lit by a series of paving uplights and or adjacent pole lights.





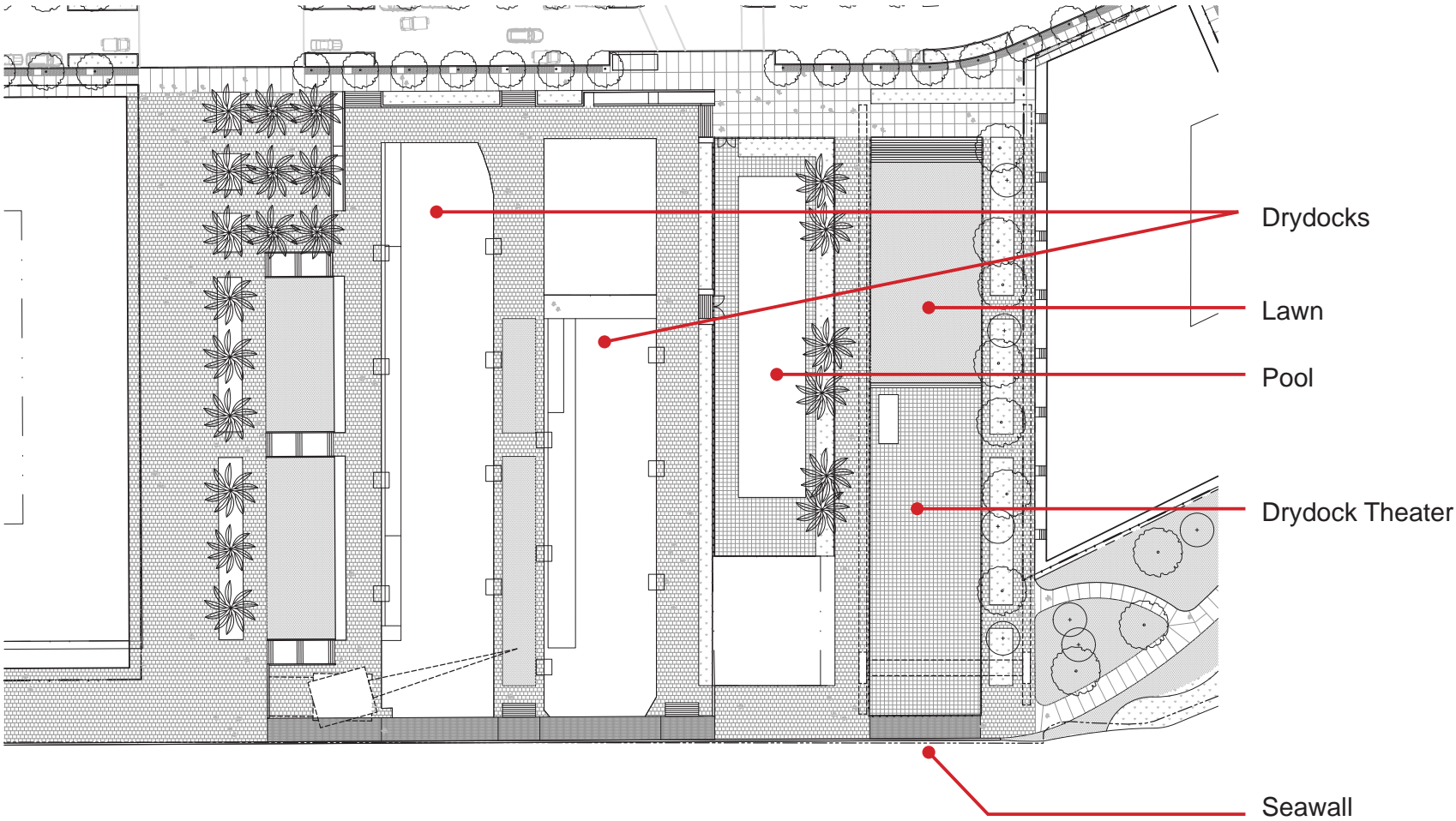
VI.2 PRESERVATION & INTERPRETATION OF HISTORIC RESOURCES

Figure VI.2.f - DRYDOCKS



Drydocks

Of the four original drydocks, two will be flooded to provide boat access to the river, one will be filled and raised to accommodate a community pool and restaurant, and one will become a sunken outdoor theatre for performances and films, the perimeter walls of this one will be raised by approximately 5'. The large doors for the two flooded drydocks will be permanently fixed in an open position, all walls will be cleaned and parged to a smooth finish. To insure access down to the water, floating docks and associated ramps will be installed. The large doors of the drydock theatre will be permanently closed to prevent water from infiltrating the space, provisions for industrial scaled sump-pits will be provided in the event of water seeping into the space. The floor of the drydock will be resurfaced and a sloping great lawn will be installed. A minimum of two accessible routes will be provided. At the perimeter of all drydocks where there is a vertical drop of more than 30 inches, a decorative guardrail will be provided.



Seawall

The existing concrete seawall will be restored and a guardrail will be mounted adjacent to the seawall for fall protection along its entire length.



VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.a - PUBLICLY-ACCESSIBLE PARKS AND OPEN SPACES: KEY PLAN



PRINCIPAL OPEN SPACES

- P1 KNOLL, ADJACENT WETLANDS
- P2 RIVERFRONT PARK, TRAIL
- P3 FARM
- P4 LINEAR R.R. PARK (SOUTHERN)
- P5 LINEAR R.R. PARK (NORTHERN)
- P6 COMMUNITY PARK
- P7 DIAGONAL PARK TO DRY DOCK
- P8 SOUTH GREEN
- P9 WETLAND PARK, TRAIL
- BLOCK 14: DRYDOCK PLAZA

The open spaces shown on this plan provide a combination of semi-public and fully-public amenities. The swimming pool and boat house are semi-public and it is envisioned that access would be allowed for members who have purchased a fixed-term pass or for individuals who have purchased a per-day use pass. These amenities are designed to serve first the residents of the immediate neighborhood and second the County and City residents who do not live in the neighborhood. The boat docks provide public access to the water, and a portion of docks will be reserved for the boat house operator and boat house members. Visitors will be able to rent small non-motorized watercraft and explore the Napa River and its estuaries. Fully public amenities include restrooms, restaurant/bar and an amphitheater. Special events taking place in the amphitheater may require an entrance fee; this will be determined by the organizing party and/or neighborhood management. The special events may include outdoor movies, small musical concerts, theatre productions, and other cultural events.

KEY TO LAND USES

- Publicly Accessible Parks and Open Space
- Farm



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VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.b - P1: KNOLL, ADJACENT WETLANDS



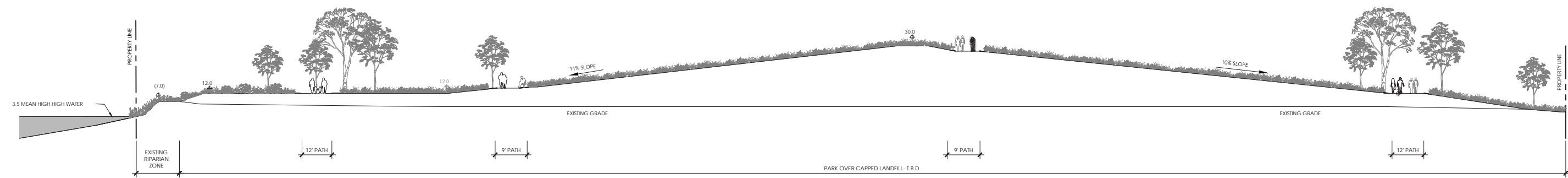
- Turf / Lawn
- Groundcovers + Shrubs
- Biofiltration Planting
- Riparian Planting
- Water





VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

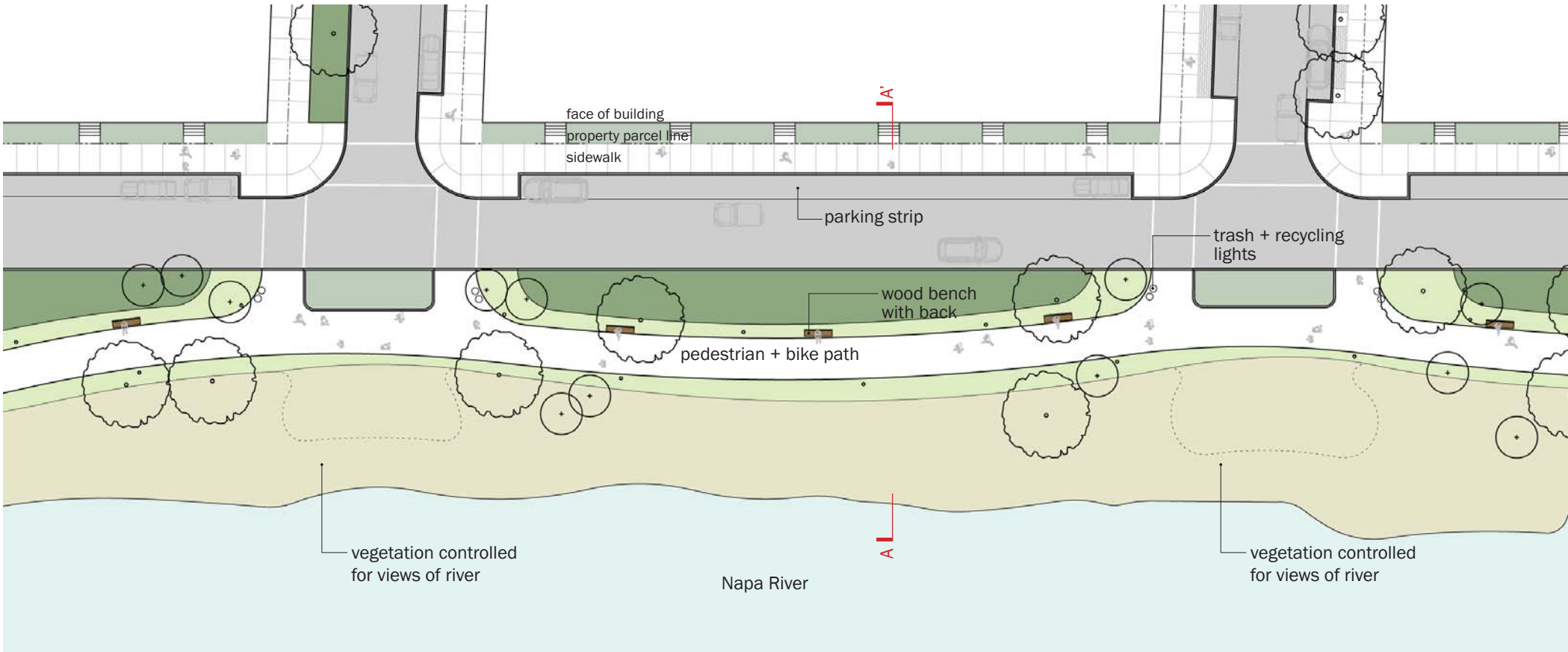
Figure VI.3.c - P1: KNOLL, ADJACENT WETLANDS - SECTION



SECTION A - A'

VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.d - P2: RIVERFRONT PARK & TRAIL

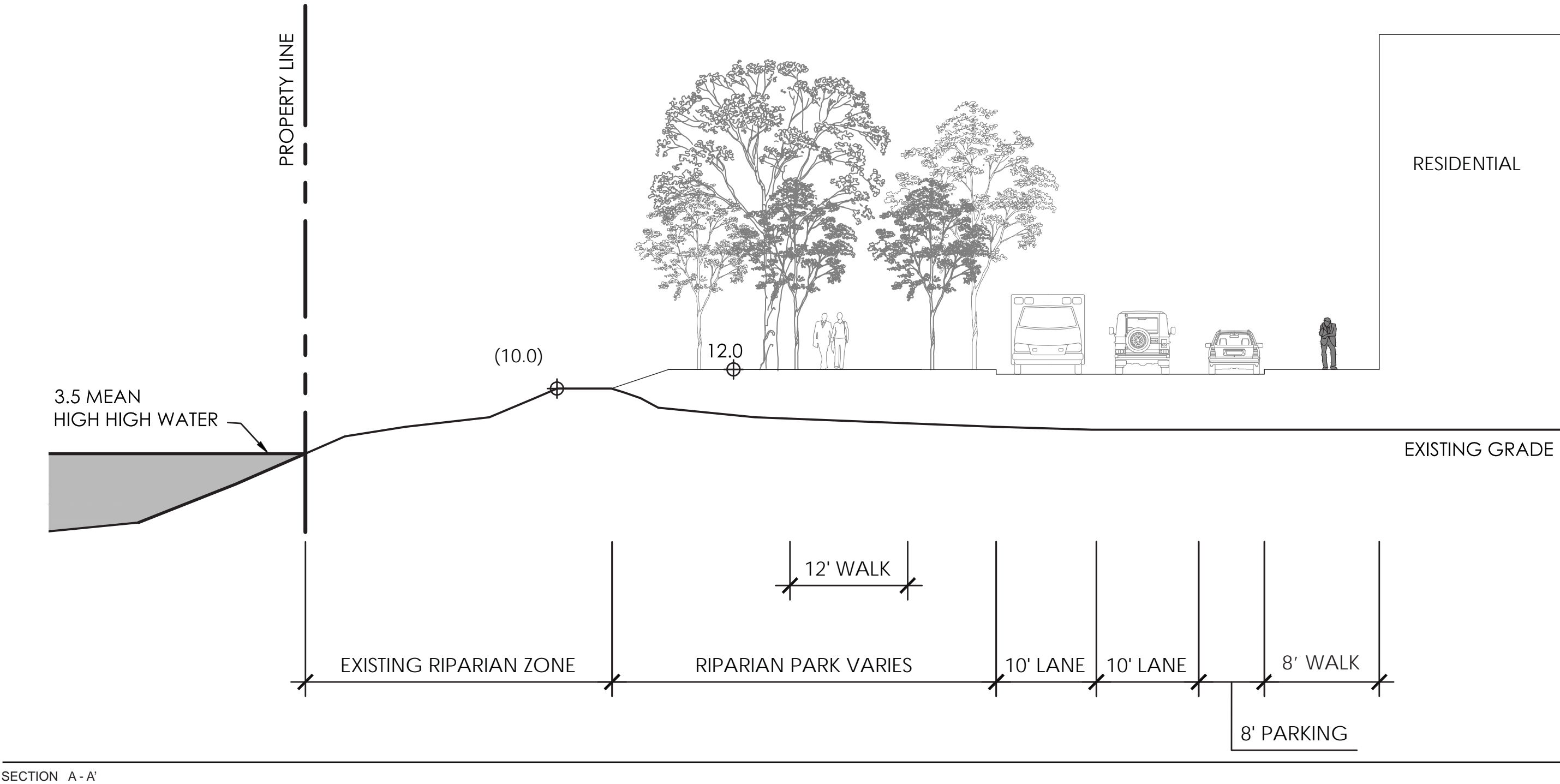


- Turf / Lawn
- Groundcovers + Shrubs
- Biofiltration Planting
- Riparian Planting
- Water



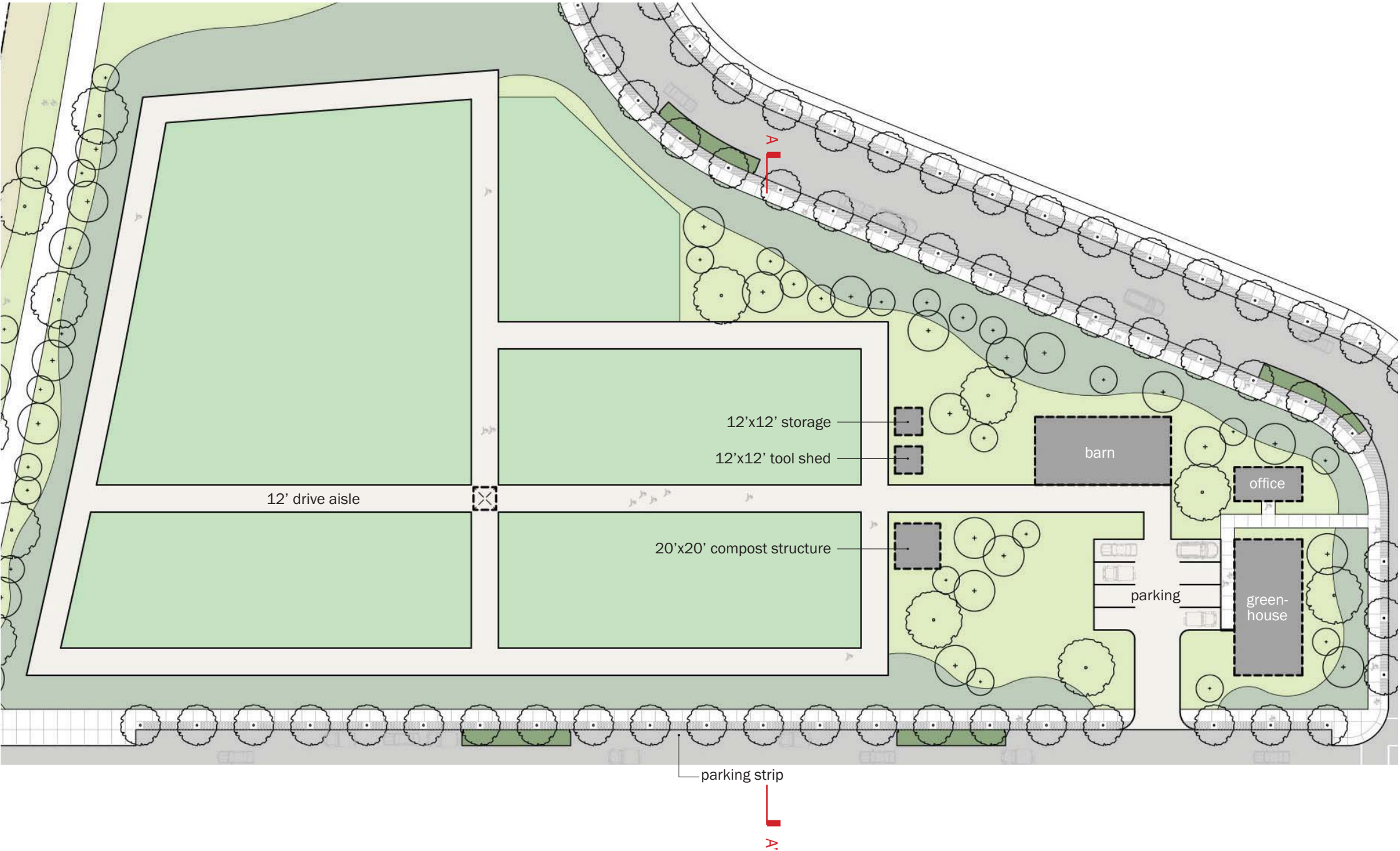
VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.e - P2: RIVERFRONT PARK & TRAIL - SECTION



VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.f - P3: FARM

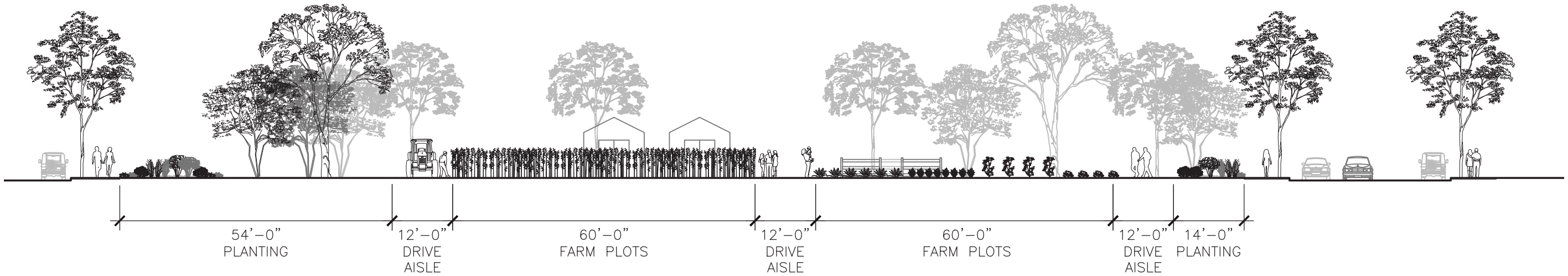


- Turf / Lawn
- Groundcovers + Shrubs
- Farm Plots
- Biofiltration Planting
- Meadow Planting



VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

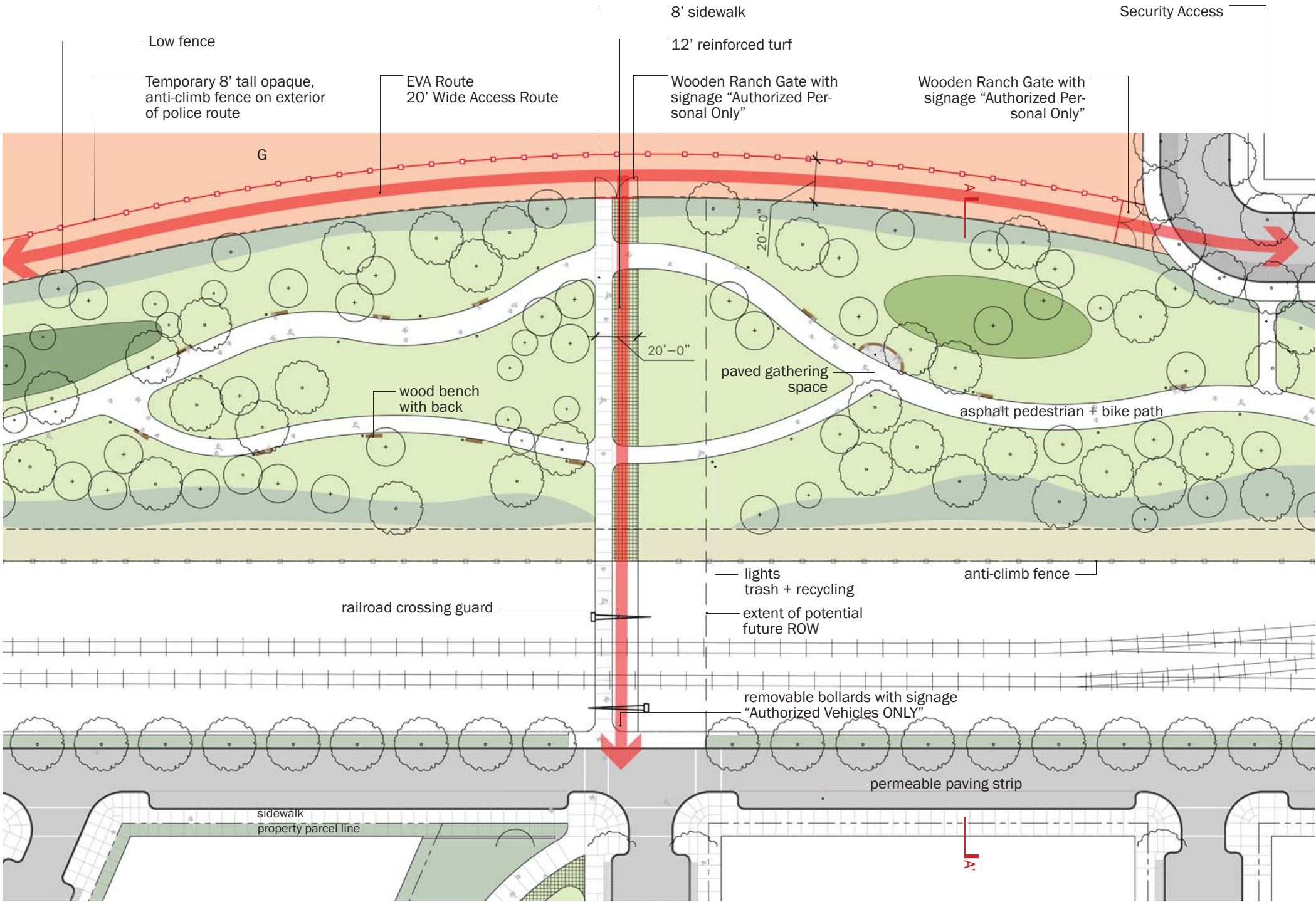
Figure VI.3.g - P3: FARM - SECTION



SECTION A - A'

VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.h - P4 & P5: RAILROAD PARK

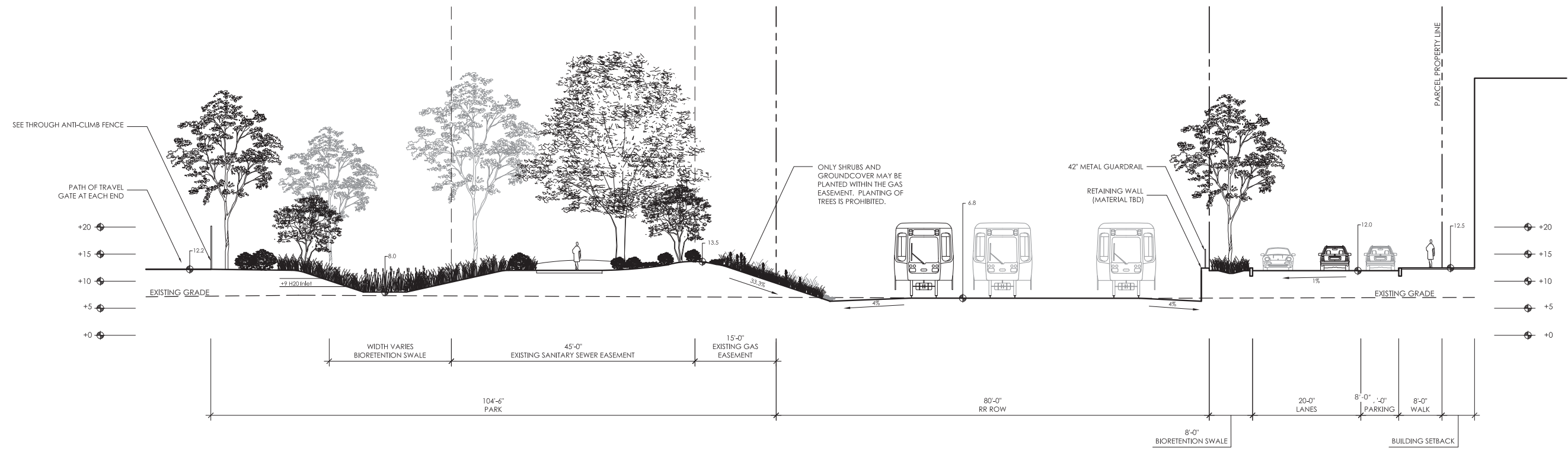


- Opaque Fence
- ↔ EVA Route
- Reinforced Turf for Fire Truck
- Turf / Lawn
- Groundcovers + Shrubs
- Meadow Planting
- Biofiltration Planting
- Non-Residential Development Parcel



VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

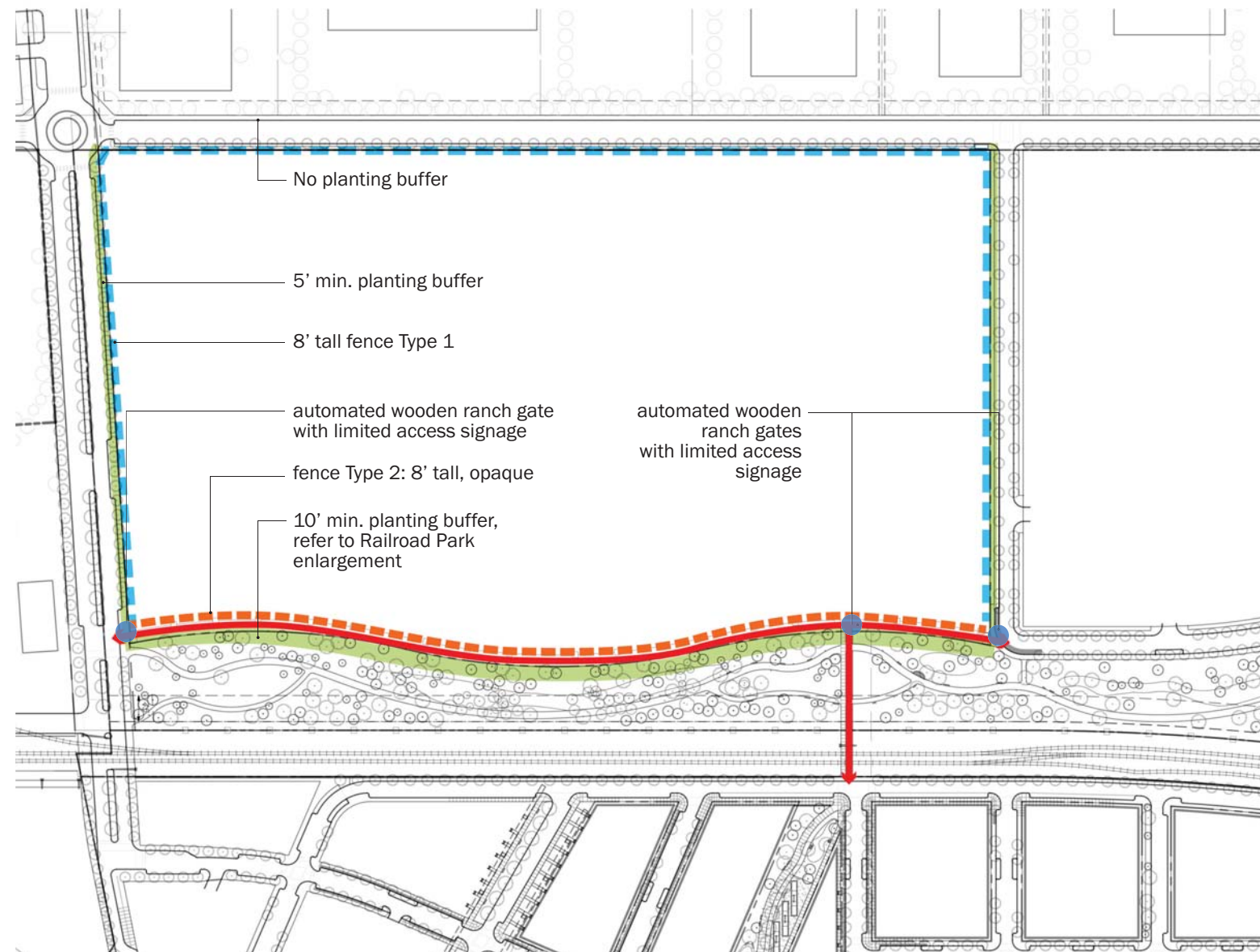
Figure VI.3.i - P4 & P5: RAILROAD PARK - SECTION



SECTION A - A'

## VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.j - SCREENED EDGE CONDITIONS



*Planting Buffer Concept:*

A repeating massing (approximately 30 feet long) of native shrubs of various heights (low: 18 to 24 inches; medium: 3 to 5 feet tall; high: 6 to 9 feet) that will provide seasonal interest (flowers / fruit) or year around interest (foliage / bark). The plantings would require irrigation only until established (3 to 5 years) and then should be able to survive with whatever nature provides.

Tall shrubs:

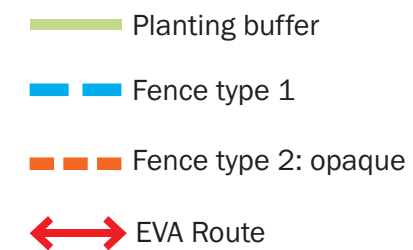
- *Arctostaphylos* Manzanita 'Dr. Hurd'
- *Ceanothus* 'Concha', Dark Star", Frosty Blue'
- *Dendromecon* *harfordii*
- *Heteromeles* *arbutifolia*

Medium shrubs:

- *Arbutus Unedo* var 'Elfin King'
- *Arctostaphylos bakeri* 'Louis Edmunds', A. 'John Dourley', A. 'Sunset'
- *Ceanothus* 'Joyce Culter', 'Julia Phelps'
- *Cistus x skanbergii*, C. 'Victor Reiter'
- *Eriogonum giganteum*
- *Rhamnus californica* 'Mound San Bruno'
- *Salvia Clevelandii* 'Allen Chickering', 'Winifred Gilman';

Small shrubs:

- *Arctostaphylos edmundsii* 'Carmel Sur'
- *Artemisia californica* 'Canyon Gray' or 'Montara'
- *Cistus* 'Sunset'; *C. salviifolius*



Note:  
Selected fence type to be non-climbable.



Fence type 1  
Wood panels  
Natural wood color or painted white



Fence type 2  
Chain link fence with privacy slats  
Medium gray or green color slats



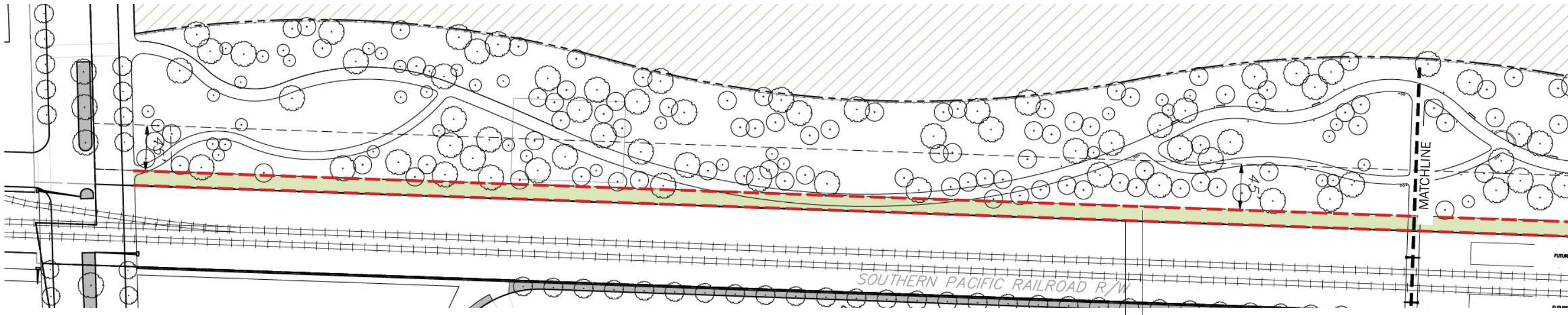
Automated wooden ranch gate for entry to police access route



VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.k - PG&E EASEMENT AREA PLANTING

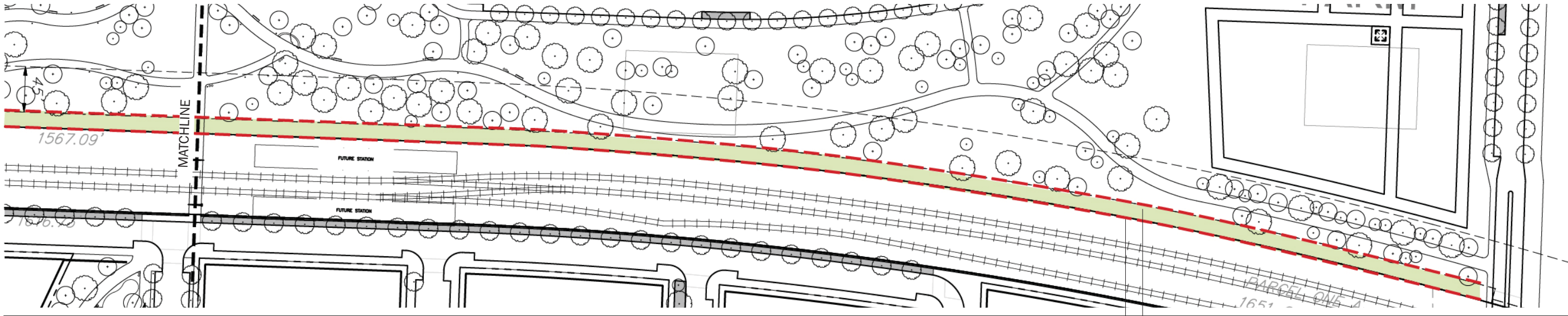
The 15' wide PG&E gas pipeline easement is located just east of the railroad and begins on the railroad ROW. East of the PG&E easement is a 45' wide Sanitary & Sewer Easement. No trees shall be planted within the PG&E easement; only shrubs and groundcovers may be planted within the PG&E easement.



ENLARGEMENT 1

PG&E easement area clear of trees.  
Only shrubs and groundcover may be planted.

PG&E easement



ENLARGEMENT 2

PG&E easement area clear of trees.  
Only shrubs and groundcover may be planted.

PG&E easement



VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.I - P6: COMMUNITY PARK

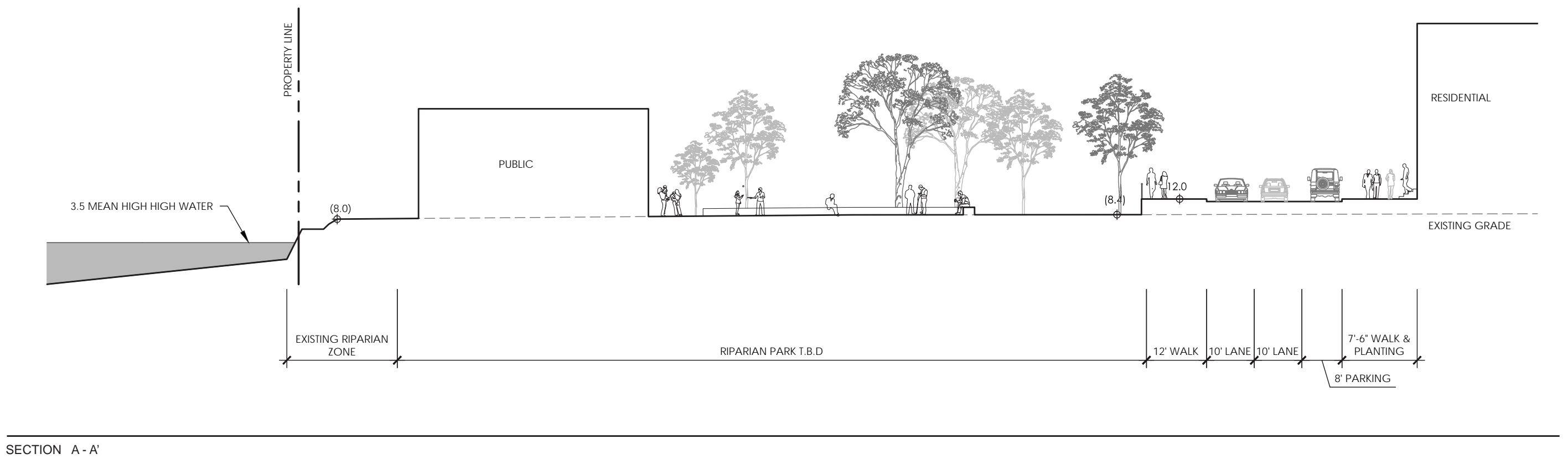


- Turf / Lawn
- Groundcovers + Shrubs
- Biofiltration Planting
- Riparian Planting
- Water



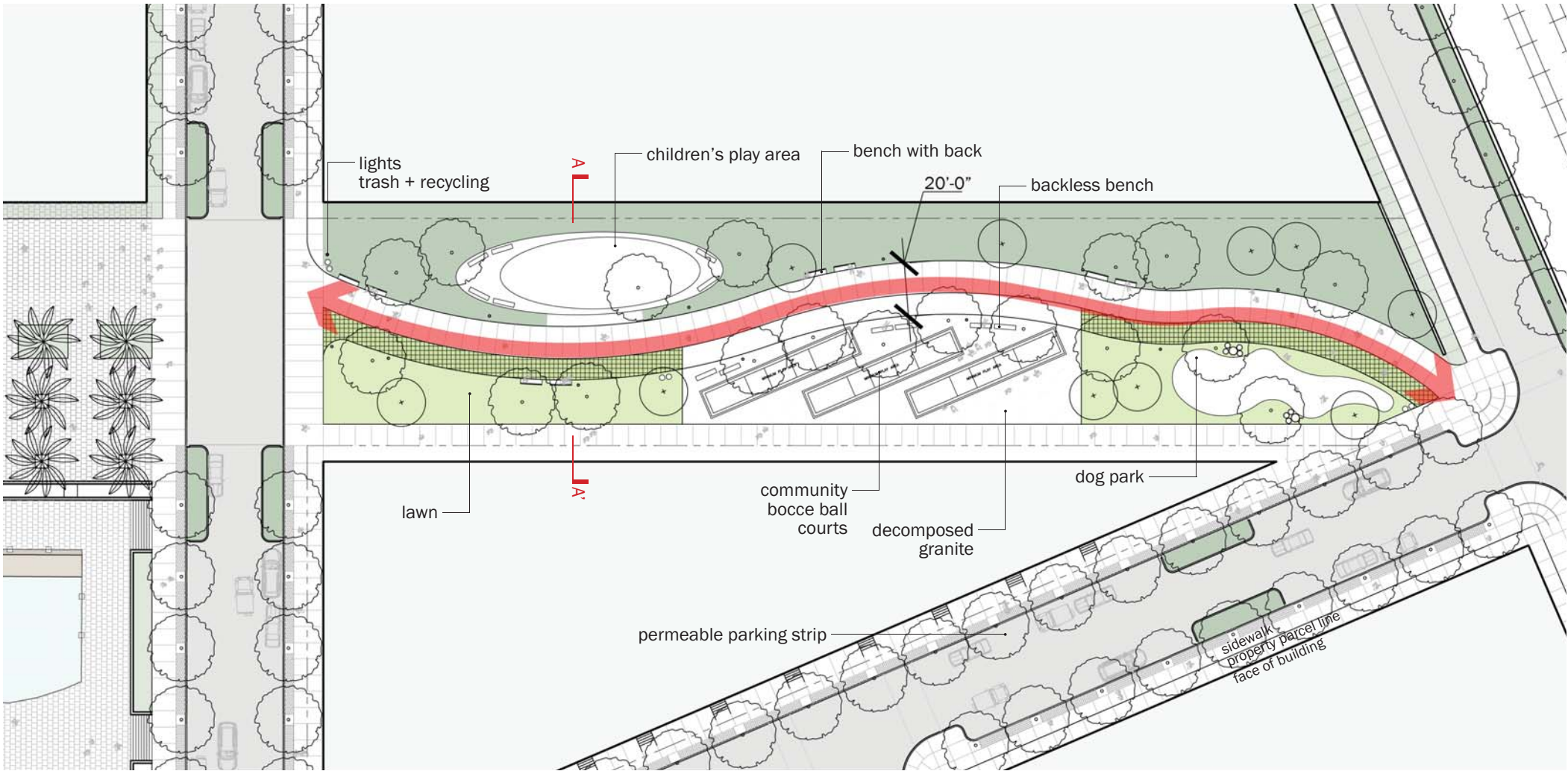
VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.m - P6: COMMUNITY PARK - SECTION



VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.n - P7: DIAGONAL PARK TO DRY DOCK

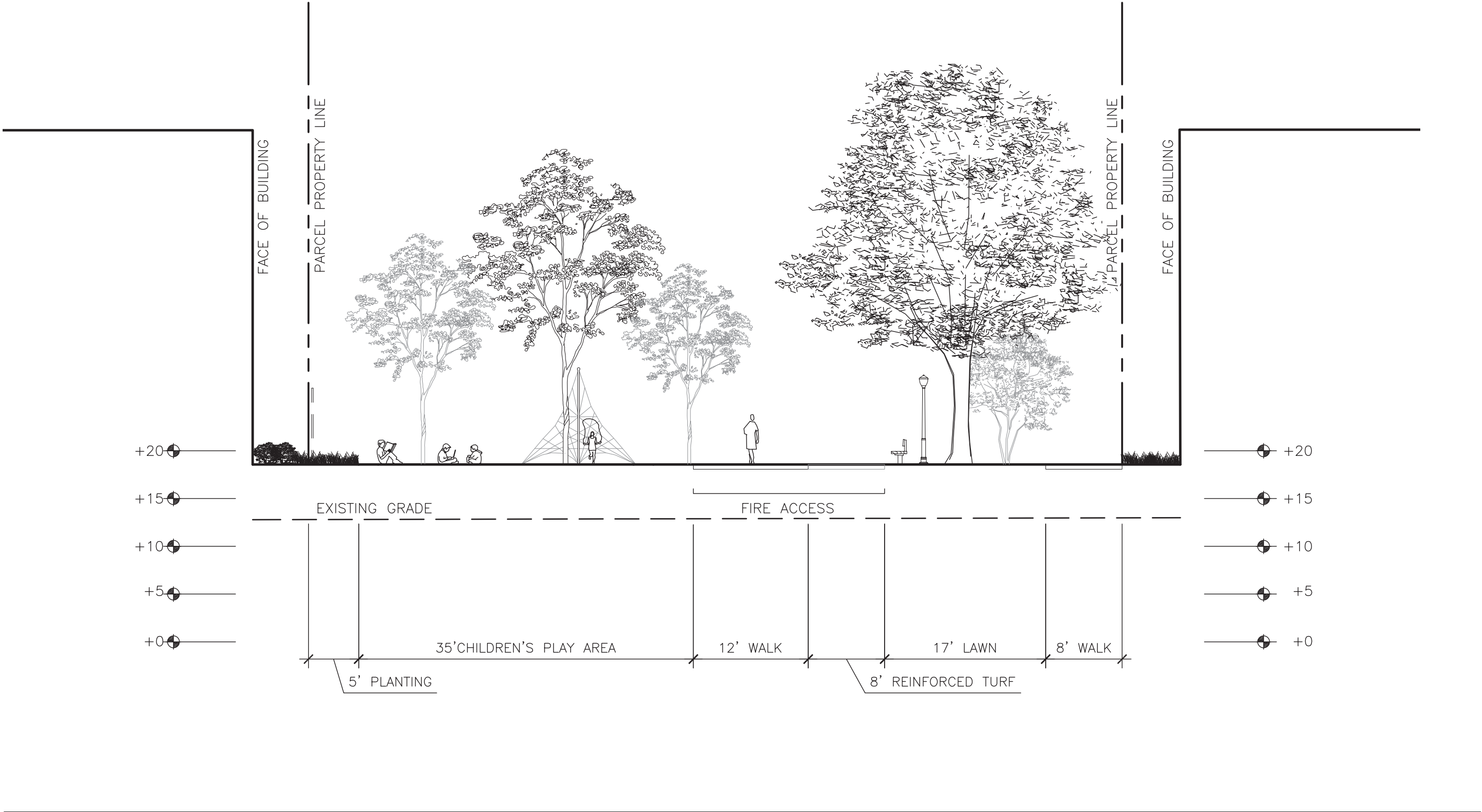


- ↔ EVA Route
- Reinforced Turf for Fire Truck
- Turf / Lawn
- Groundcovers + Shrubs
- Biofiltration Planting
- Water



VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

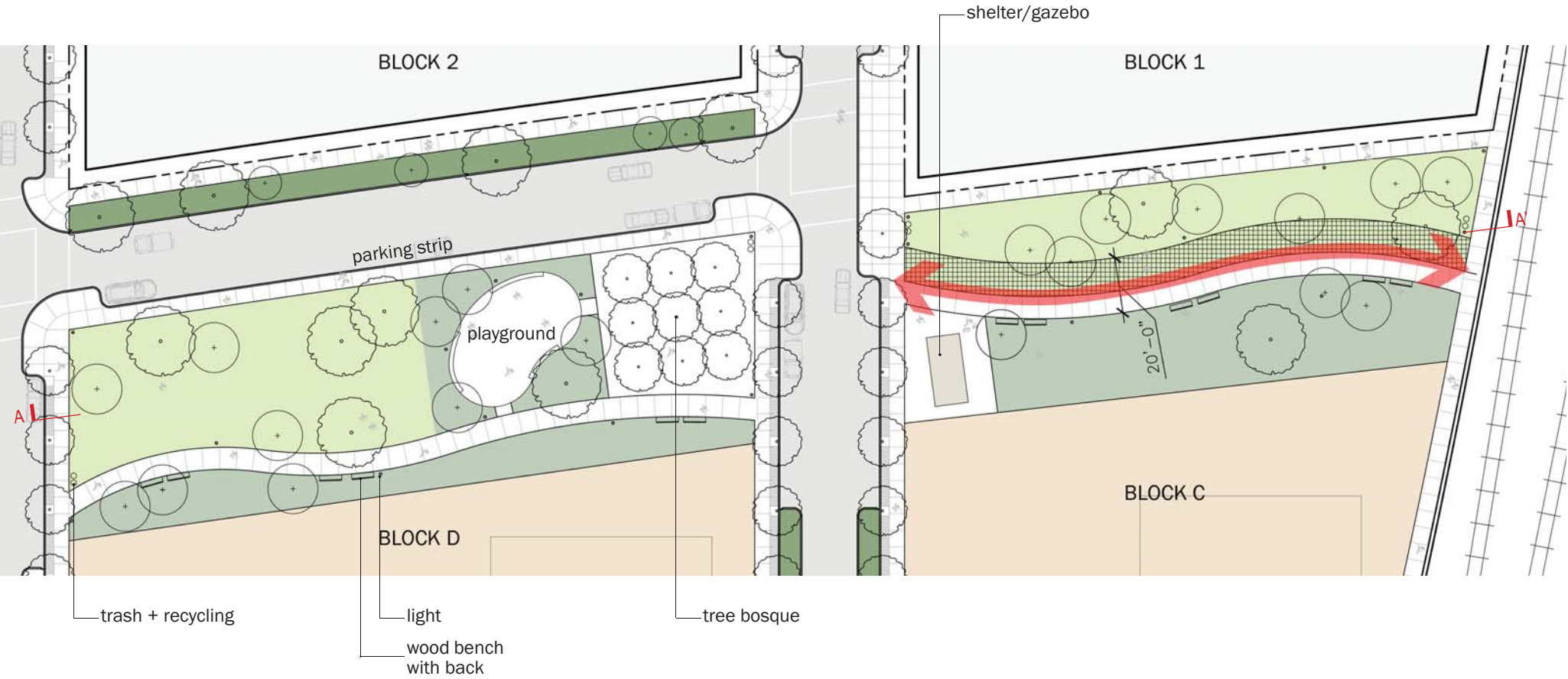
Figure VI.3.o - P7: DIAGONAL PARK TO DRY DOCK - SECTION



SECTION A - A'

VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.p - P8: SOUTH GREEN

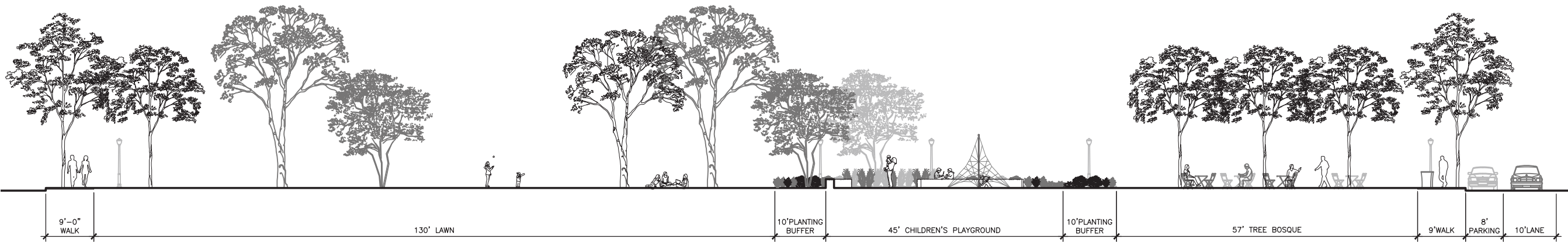


- EVA Route
- Reinforced Turf for Fire Truck
- Turf / Lawn
- Groundcovers + Shrubs
- Biofiltration Planting
- Non-Residential Development Parcel

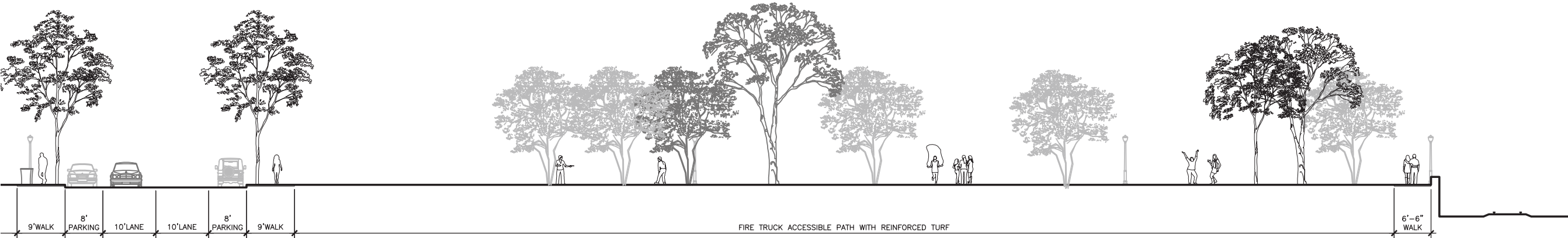


VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.q - P8: SOUTH GREEN - SECTIONS



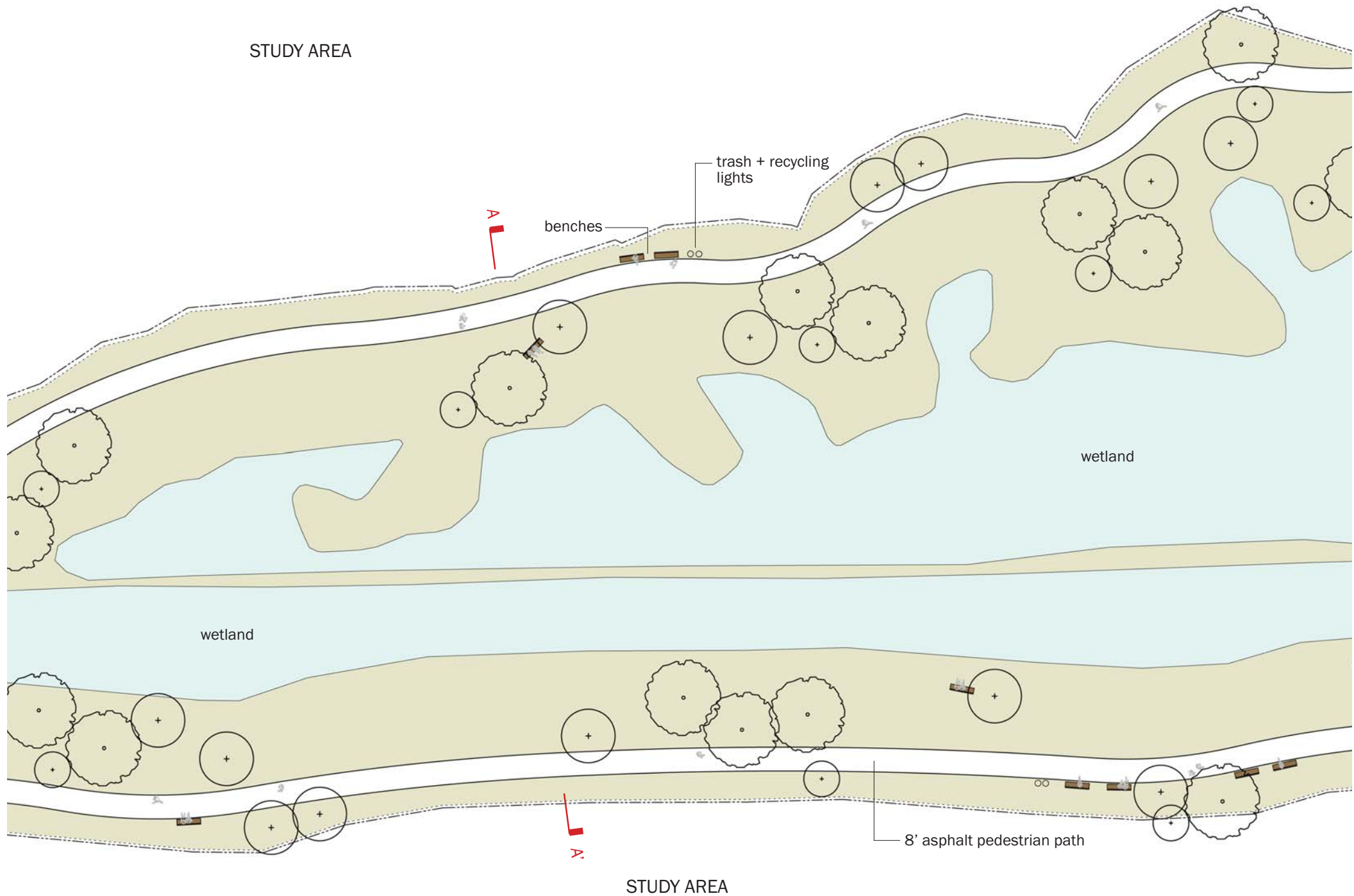
SECTION A - A' - Western Block



SECTION A - A' - Eastern Block

VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.r - P9: WETLAND PARK & TRAIL



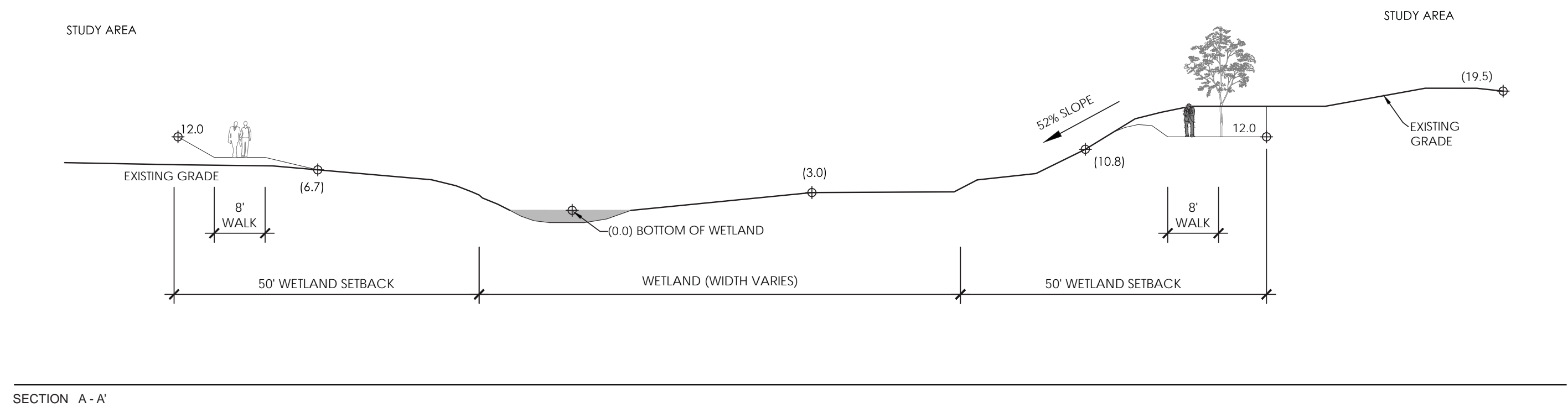
- Study Area
- Riparian Planting
- Water





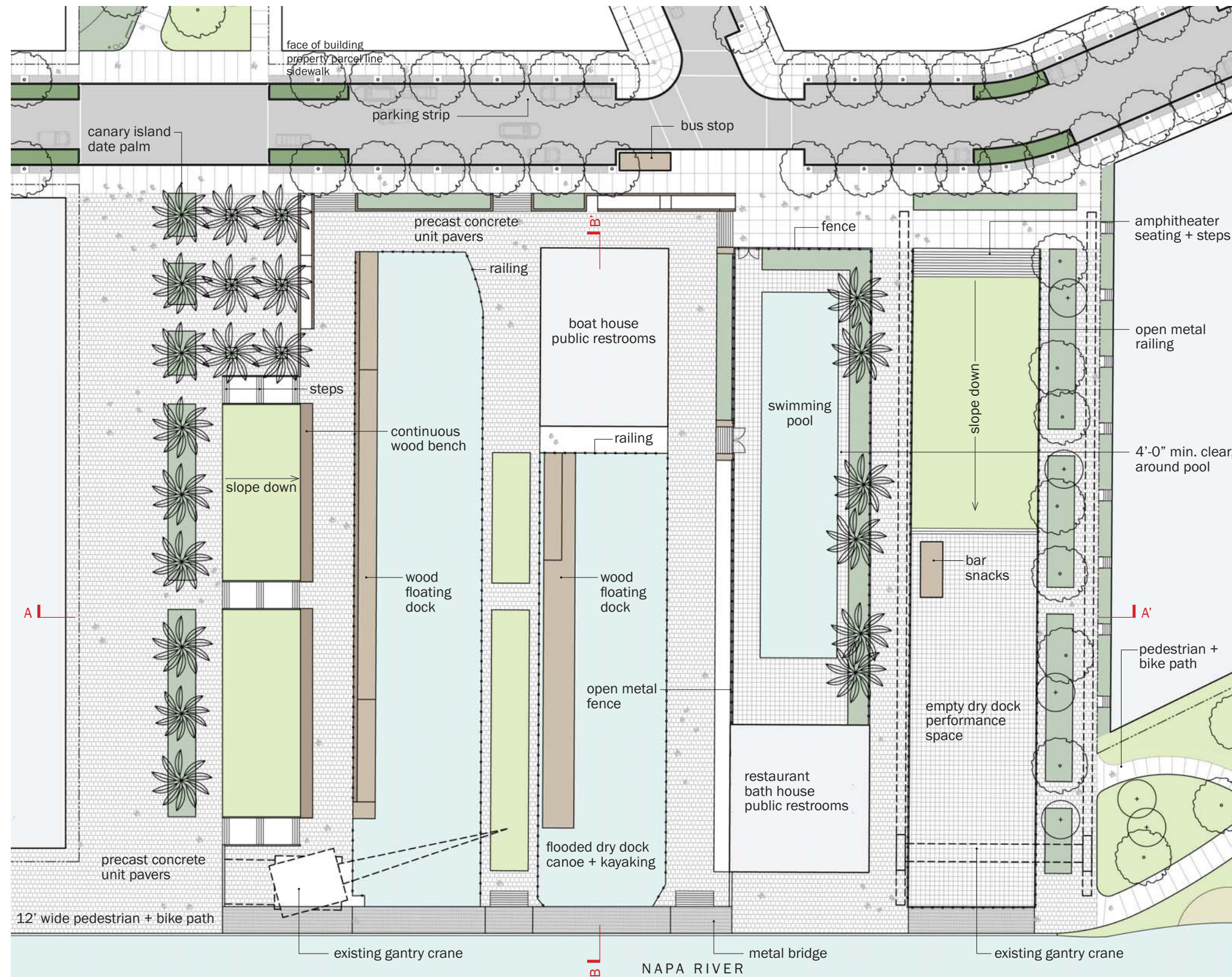
VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.s - P9: WETLAND PARK & TRAIL - SECTION

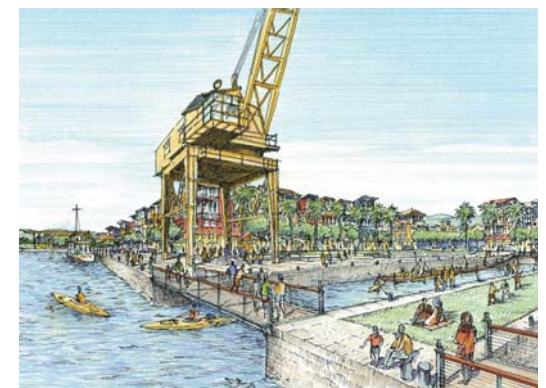


## VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.t - BLOCK 14: DRY DOCK PLAZA



The open spaces shown on this plan provide a combination of semi-public and fully public amenities. The swimming pool and boat house are semi-public and its envisioned that access would be allowed for members who have purchased a fixed-term pass or for individuals who have purchased a per-day use pass. These amenities are designed to first serve the residents of the immediate neighborhood and secondarily to serve County and City residents who do not live in the neighborhood. The boat docks provide public access to the water and a portion of docks will be reserved for the boat house operator and boat house members. Visitors will be able to rent small non-motorized and explore the Napa River and its estuaries. Fully public amenities include restrooms, restaurant/bar and an amphitheater. Special events taking place in the amphitheater may require an entrance fee; this will be determined by the organizing party and/or neighborhood management. The special events may include outdoor movies, small musical concerts, theatre productions, and other cultural events.

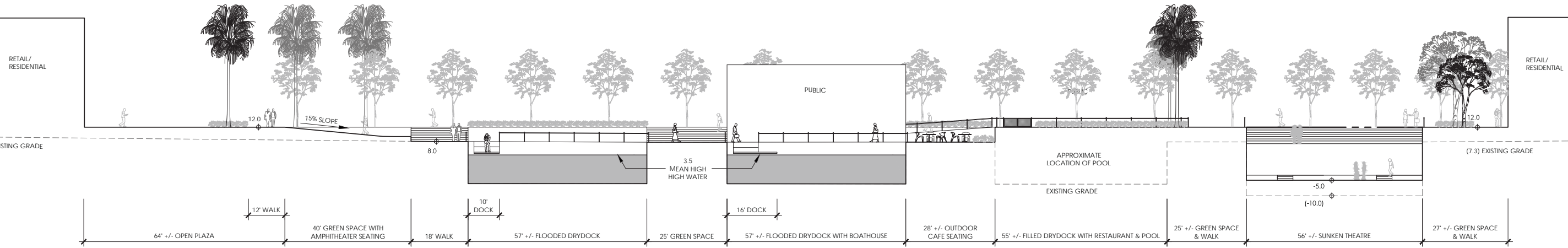


- Turf / Lawn
- Groundcovers + Shrubs
- Biofiltration Planting
- Riparian Planting
- Water

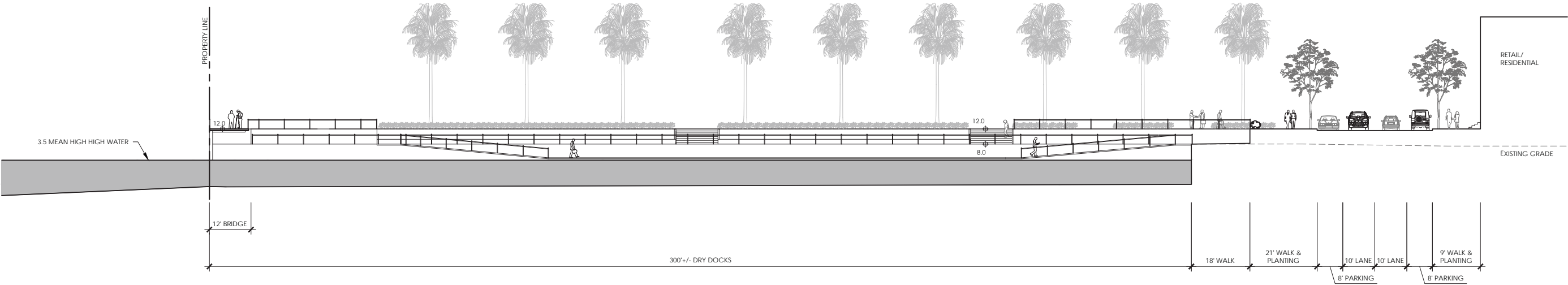


VI.3 SCHEMATIC DESIGNS OF PRINCIPAL OPEN SPACES

Figure VI.3.u - BLOCK 14: DRY DOCK PLAZA - SECTIONS



SECTION A - A'



SECTION B - B'



VI.4 TRAIL PLAN: KENNEDY PARK CONNECTION STUDY

Figure VI.4.a - EXISTING CONDITIONS





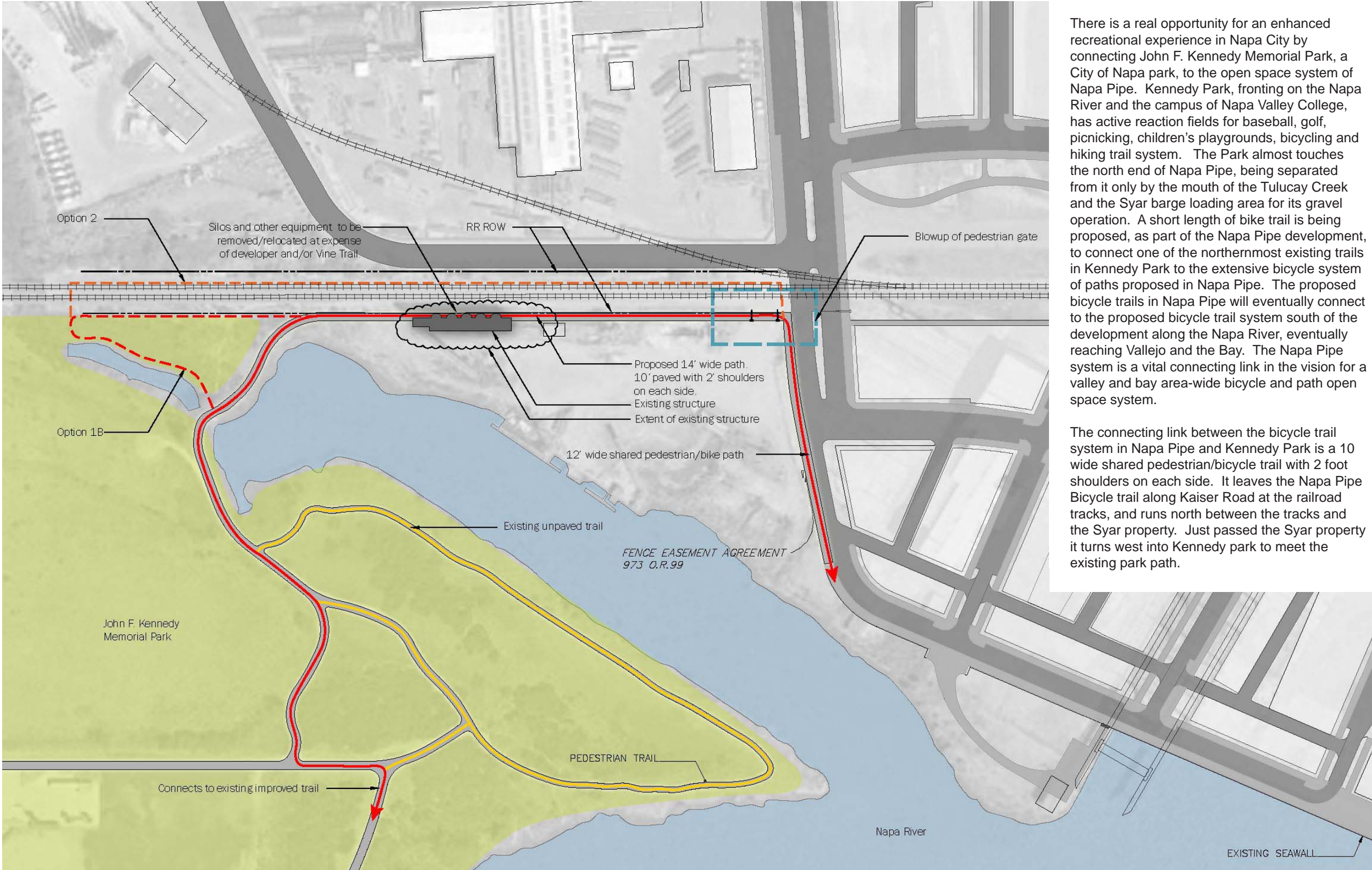
Figure VI.4.b - EXISTING CONDITIONS - VIEW





VI.4 TRAIL PLAN: KENNEDY PARK CONNECTION STUDY

Figure VI.4.c - CONTEXT

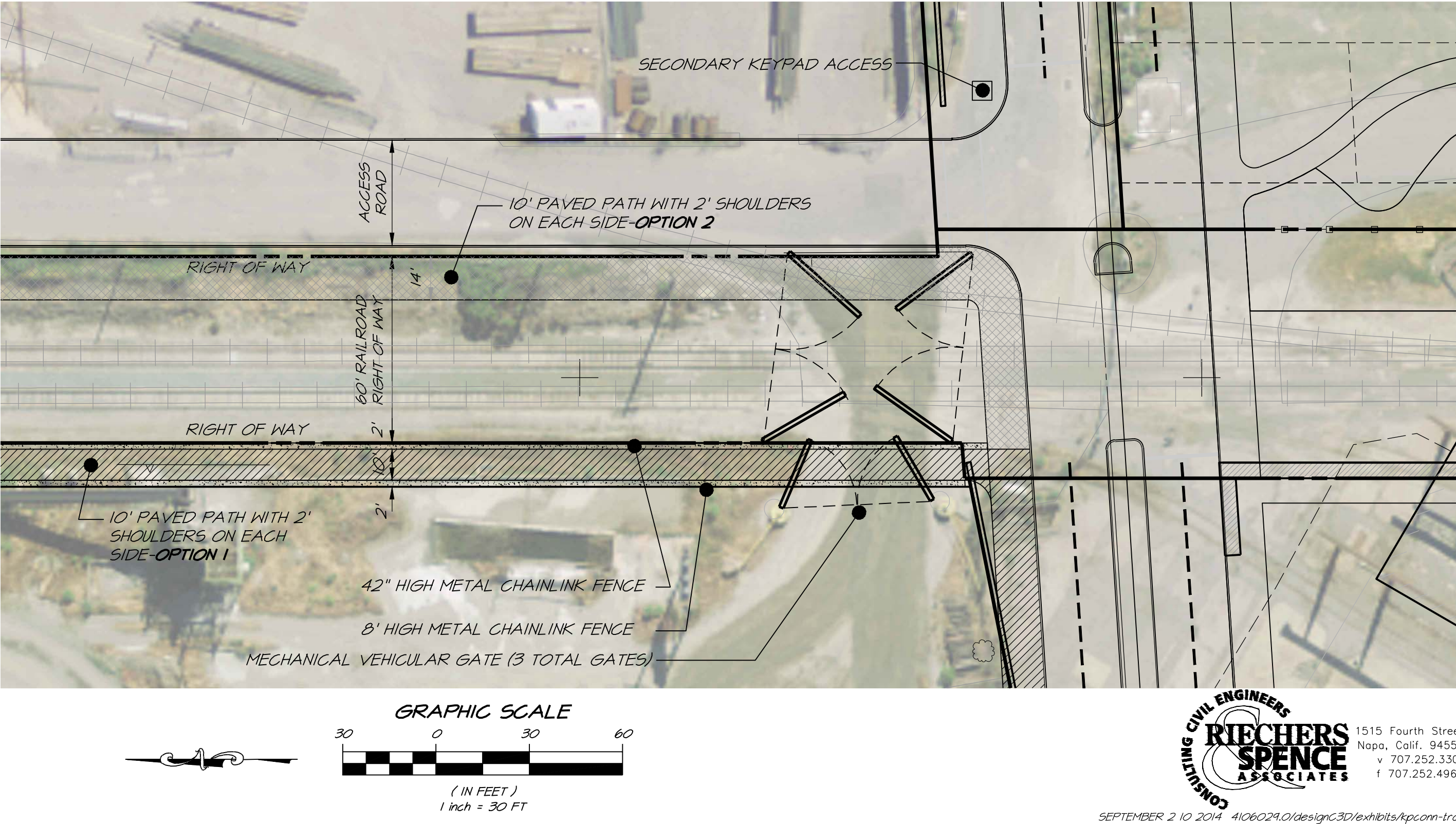


There is a real opportunity for an enhanced recreational experience in Napa City by connecting John F. Kennedy Memorial Park, a City of Napa park, to the open space system of Napa Pipe. Kennedy Park, fronting on the Napa River and the campus of Napa Valley College, has active recreation fields for baseball, golf, picnicking, children's playgrounds, bicycling and hiking trail system. The Park almost touches the north end of Napa Pipe, being separated from it only by the mouth of the Tulucay Creek and the Syar barge loading area for its gravel operation. A short length of bike trail is being proposed, as part of the Napa Pipe development, to connect one of the northernmost existing trails in Kennedy Park to the extensive bicycle system of paths proposed in Napa Pipe. The proposed bicycle trails in Napa Pipe will eventually connect to the proposed bicycle trail system south of the development along the Napa River, eventually reaching Vallejo and the Bay. The Napa Pipe system is a vital connecting link in the vision for a valley and bay area-wide bicycle and path open space system.

The connecting link between the bicycle trail system in Napa Pipe and Kennedy Park is a 10 wide shared pedestrian/bicycle trail with 2 foot shoulders on each side. It leaves the Napa Pipe Bicycle trail along Kaiser Road at the railroad tracks, and runs north between the tracks and the Syar property. Just passed the Syar property it turns west into Kennedy park to meet the existing park path.



Figure VI.4.d - VEHICULAR GATE BLOWUP





VI.4 TRAIL PLAN: KENNEDY PARK CONNECTION STUDY

Figure VI.4.e - AERIAL ENLARGEMENT

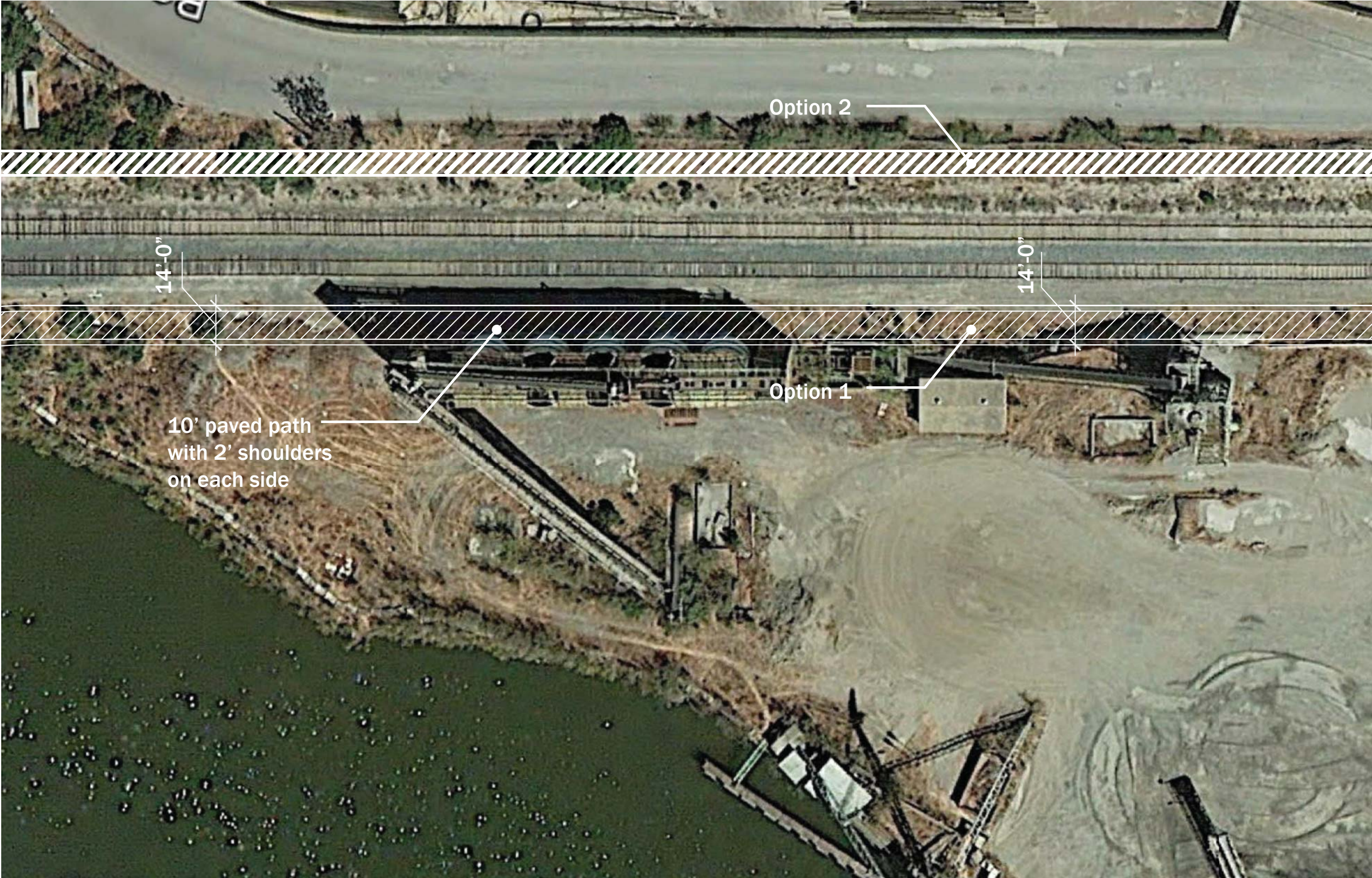
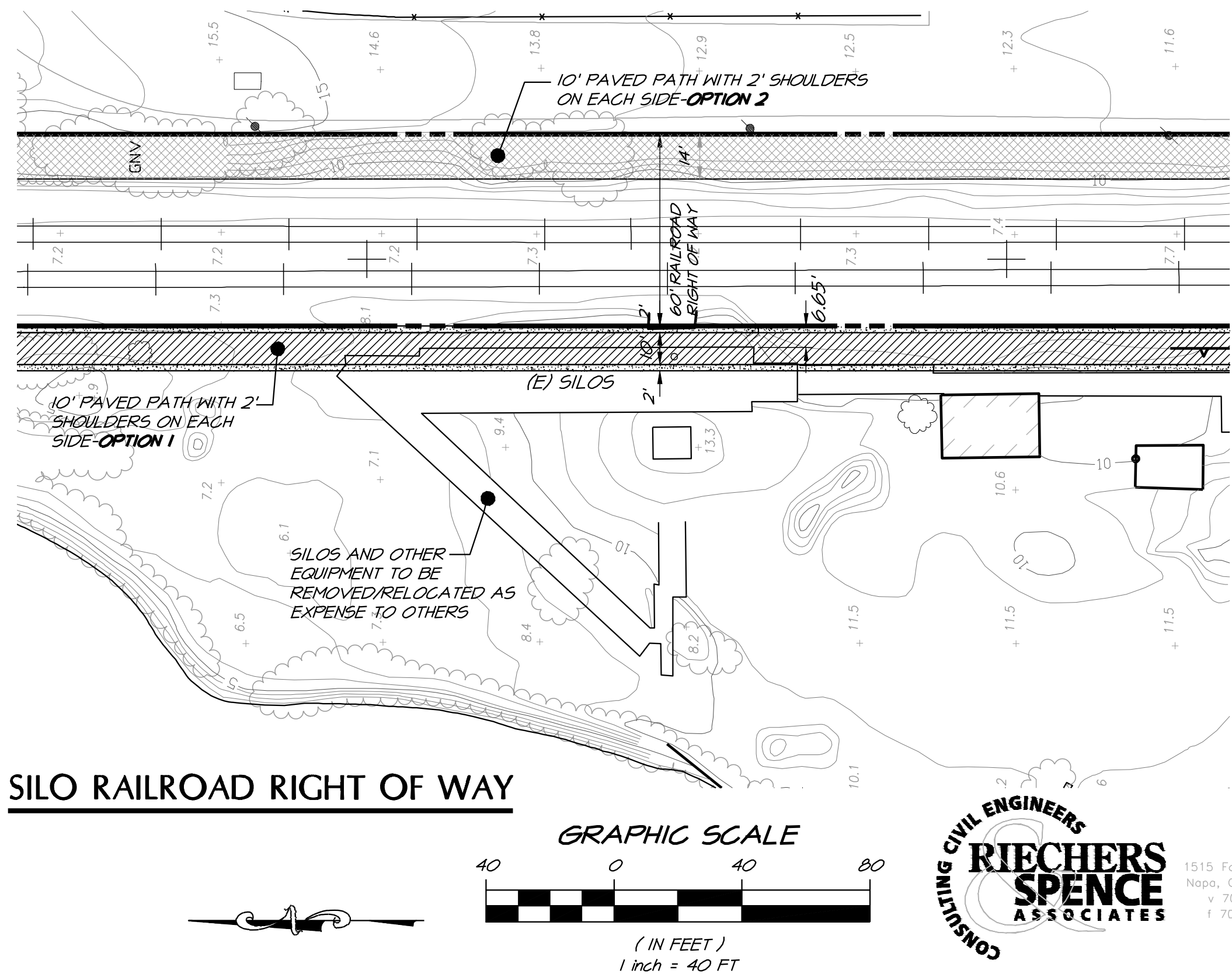


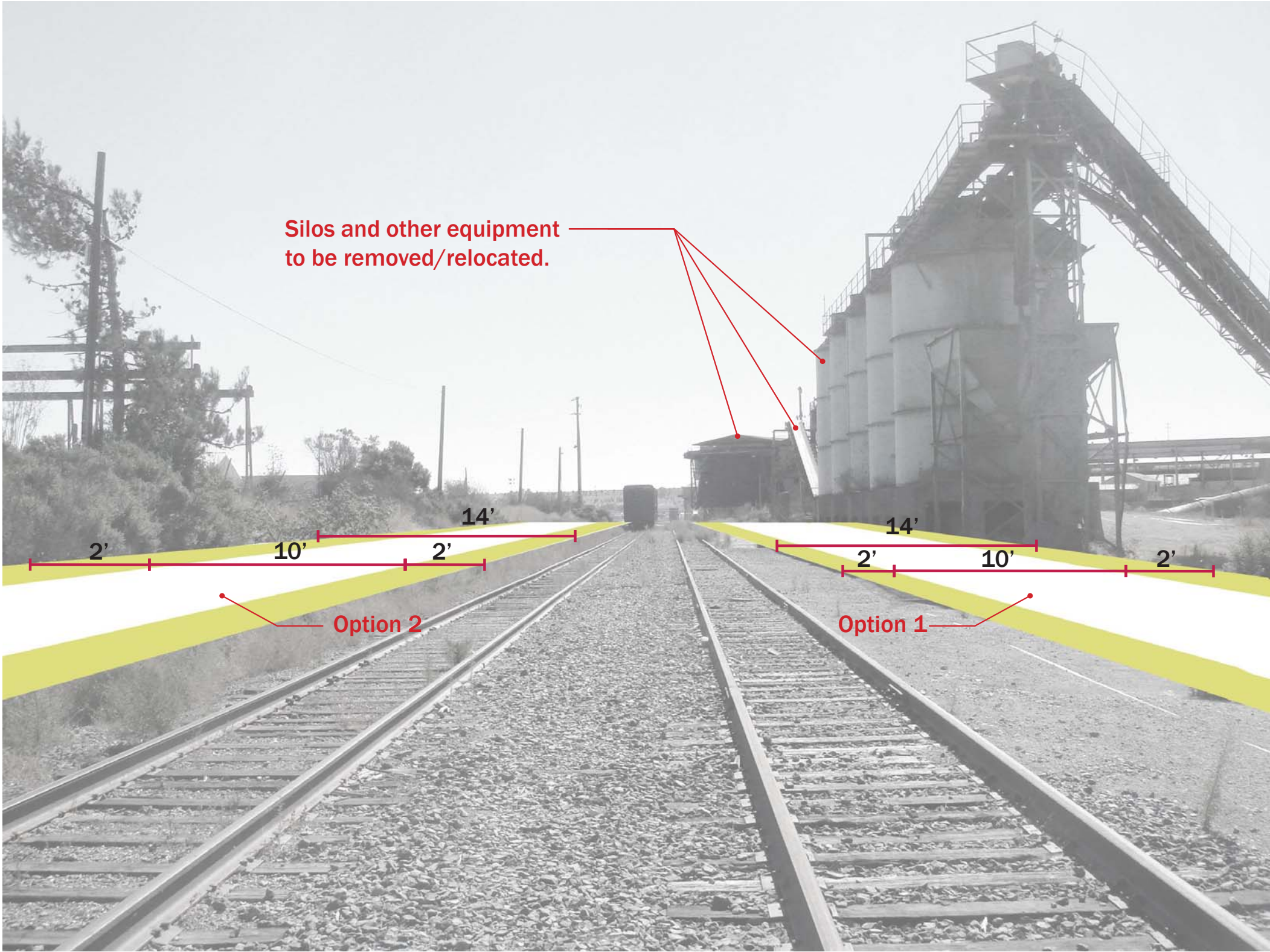


Figure VI.4.f - SURVEY



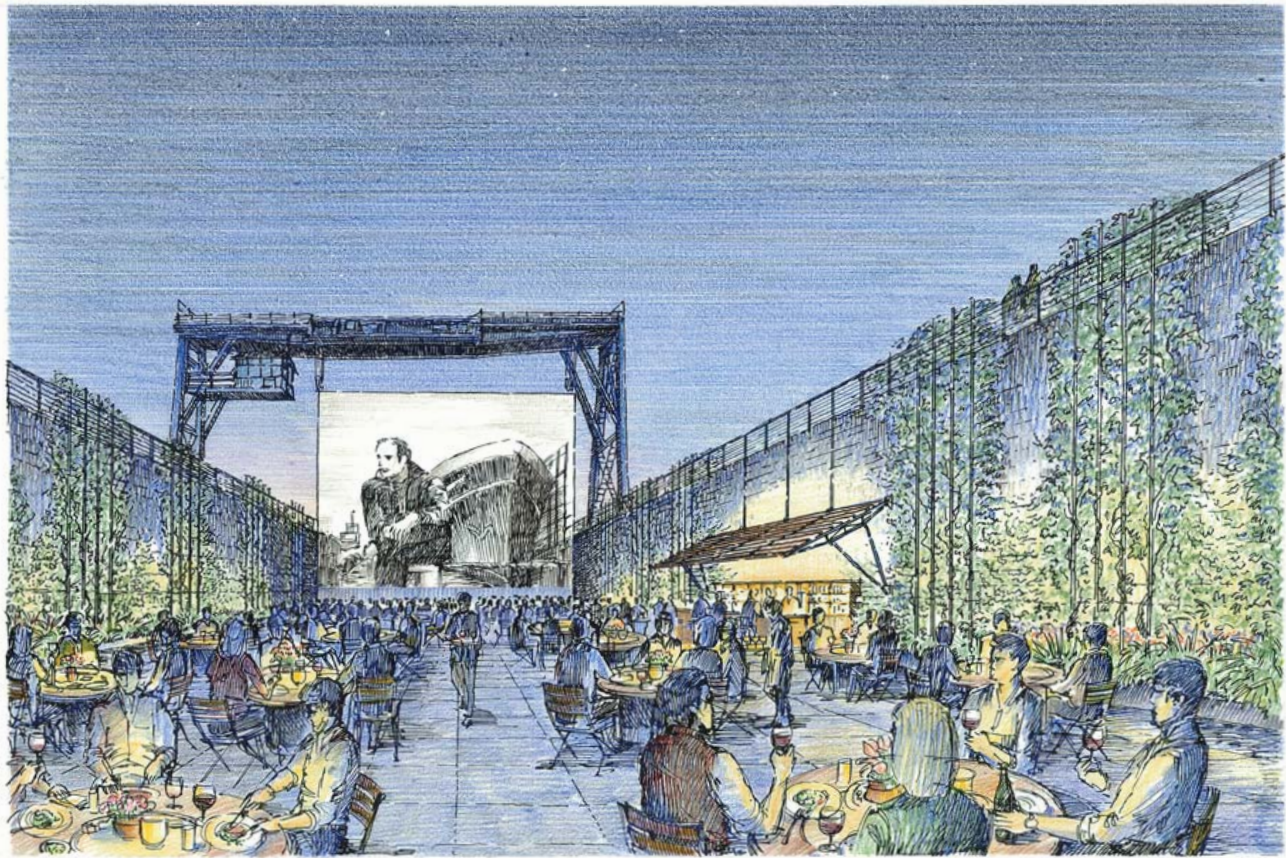
VI.4 TRAIL PLAN: KENNEDY PARK CONNECTION STUDY

Figure VI.4.g - SIMULATION





VI.5 OTHER PUBLIC BENEFITS



**The following community facilities will be located on the 63-acre parcel:**

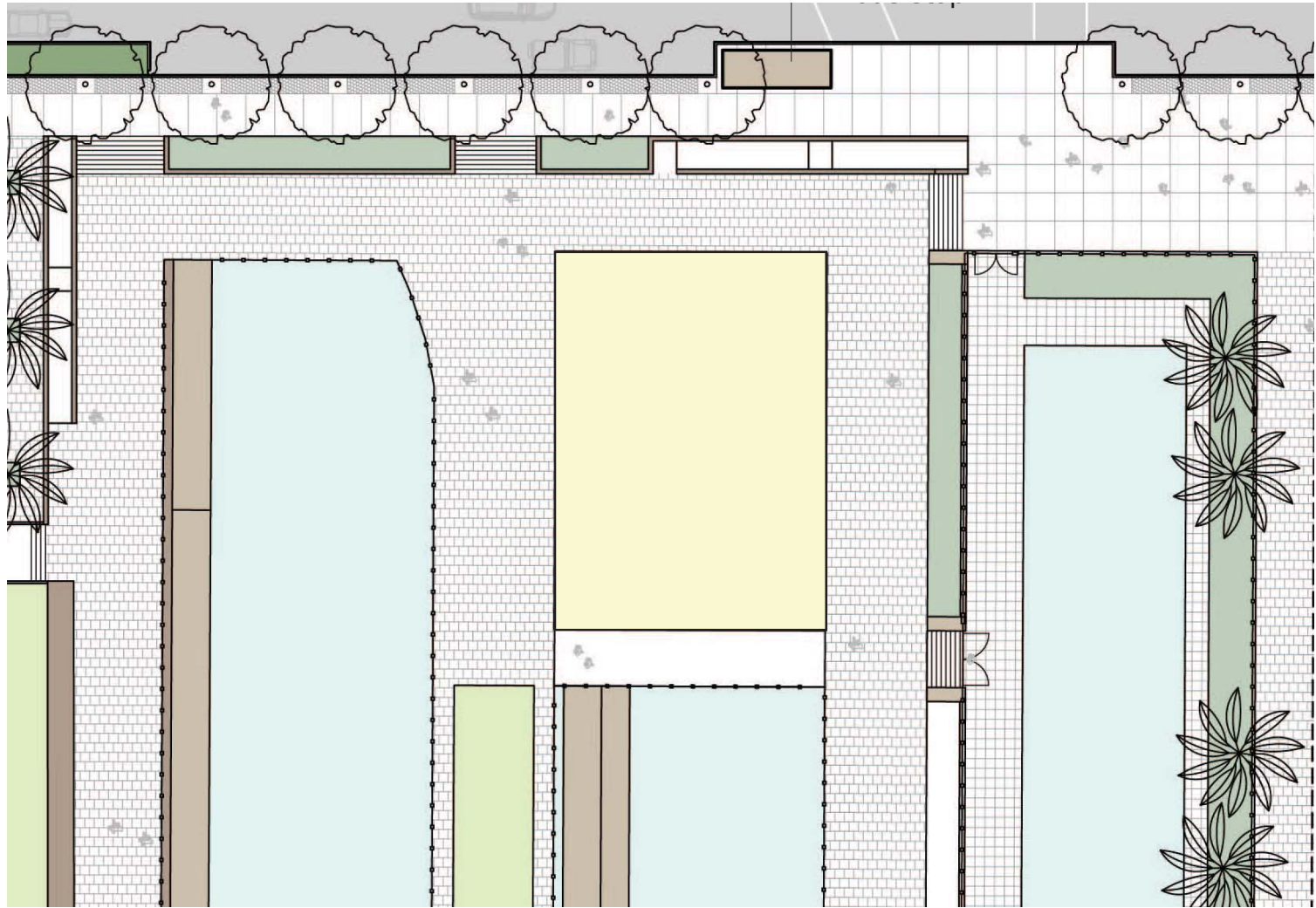
- A large, open plaza will be constructed in the vicinity of the boathouse and dry docks. The plaza will serve as a public gathering place.
- Existing overhead cranes will be refurbished and integrated into the streetscape.
- In the dry dock area, small boat harbors, a boathouse, a swimming pool and a sunken outdoor event space will be constructed.
- An existing gantry crane will be refurbished as an outdoor movie screen adjacent to the sunken event space.
- Several industrial artifacts have been retained and incorporated into the site plan to remind visitors of the site’s history and to provide visual interest within the development. The salvaged industrial artifacts include the following:
  - Existing Storage Structure
  - Bridge Crane
  - Seawall Crane
  - Gantry Crane
  - Drydocks
  - Seawall
- If requested by the emergency service providers, an emergency services building will be provided on-site as part of the Project. The building will be located along Kaiser Road at the northern edge of the 63-acre parcel, adjacent to the railroad right-of-way. Otherwise, the building may serve other community uses.
- Space has been identified for a potential day care facility. If demand requires it, a neighborhood-serving day care facility of approximately 3,600 square feet of indoor space and 4,500 square feet outdoor space will be integrated into the center of the neighborhood.
- A transit center will consist of a kiosk that provides information regarding available transit, ticket sales, and a sheltered seating area.

Refer to Figure III.1 - Site Plan Showing Open Space, Community, and Historic Facilities.



VI.5 OTHER PUBLIC BENEFITS

Figure VI.5.a - BOAT HOUSE



The boat house is a semi-public facility designed to provide access to the river, first to the residents of the immediate neighborhood, and second to those visiting from the greater extents of the County and City. As a semi-public facility the boat house would be open to members, and to individuals who use the space on a paid, per-day basis. The facility would provide space for the storage of both club and private, small, non-motorized watercraft. These watercraft would likely include row boats, rowing shells, paddleboards, and kayaks.

**POSSIBLE AMENITIES:**

- Boat Ramp
- Public and Private Dock Space
- Small Craft Storage Space
- Locker Rooms
- Public Restrooms
- Group Meeting Space
- Coaches Office
- Rental Office
- Training Facilities

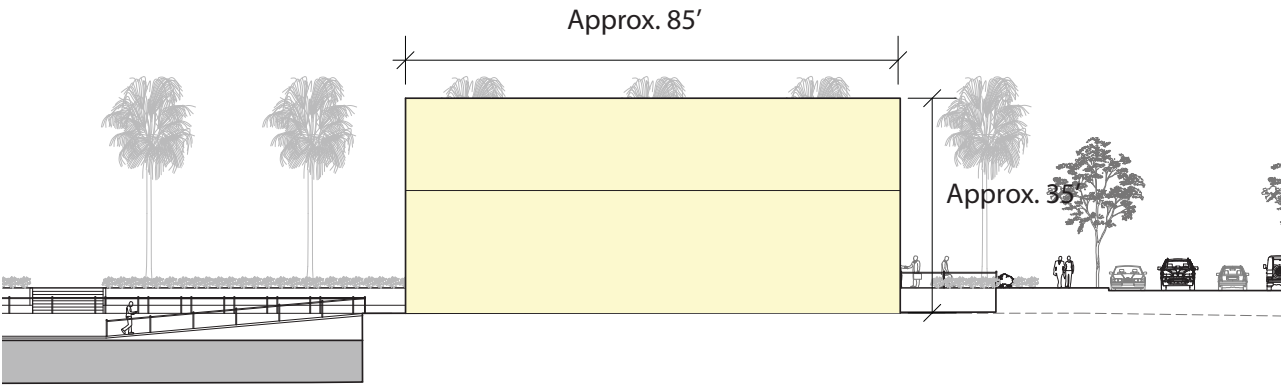
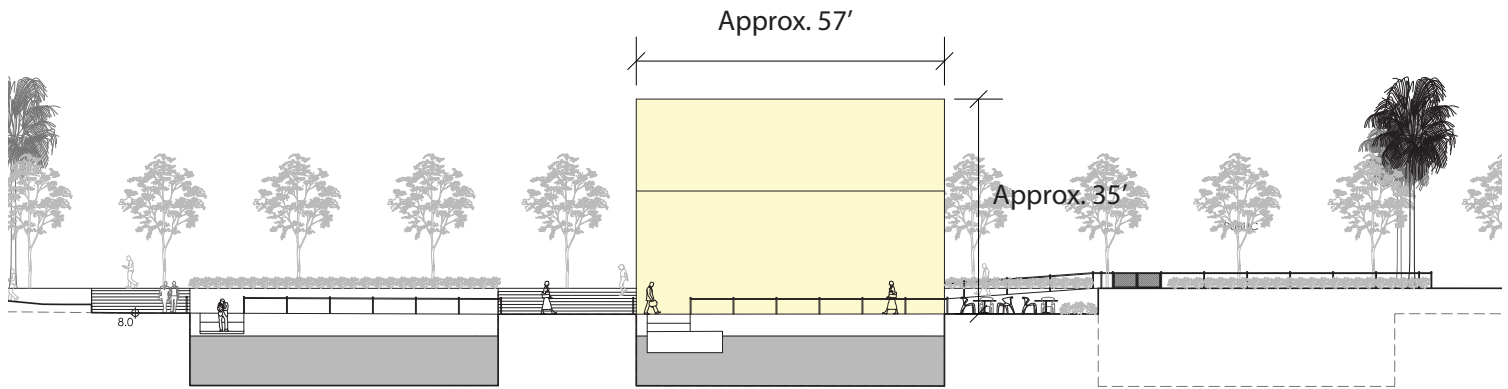
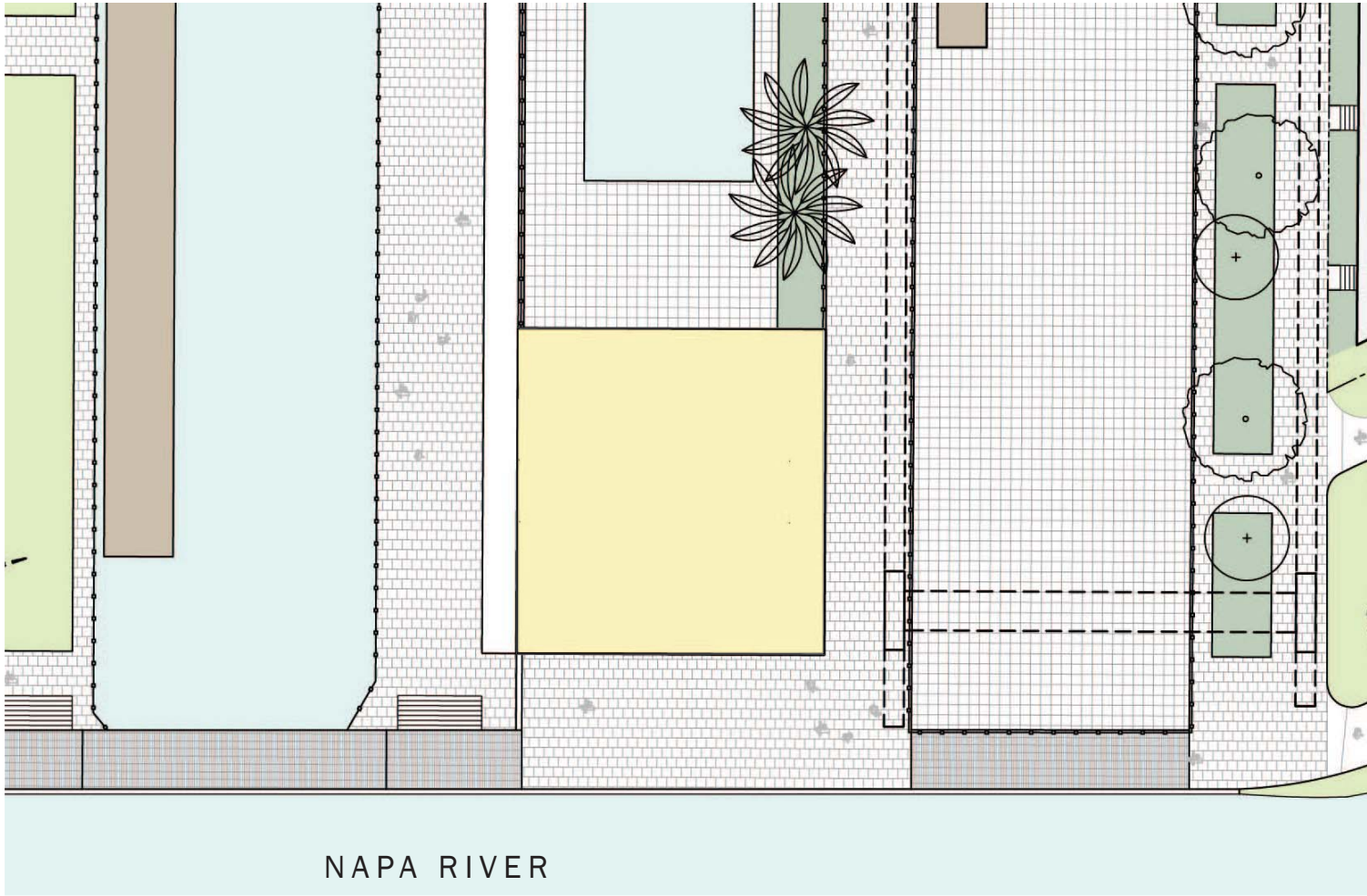




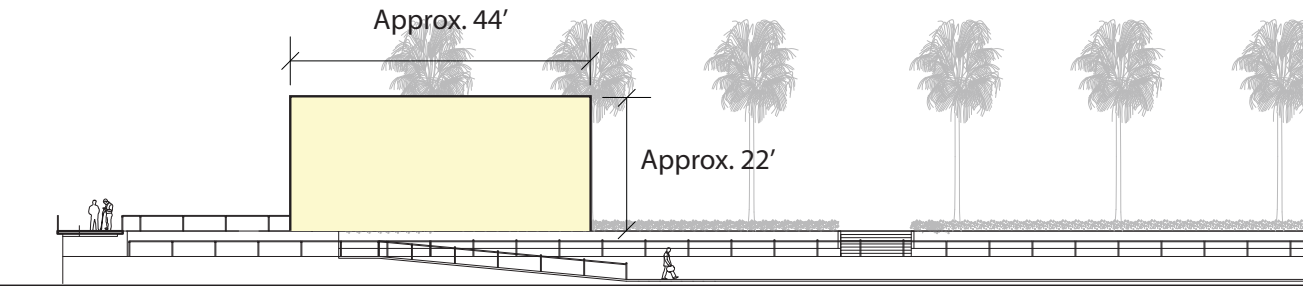
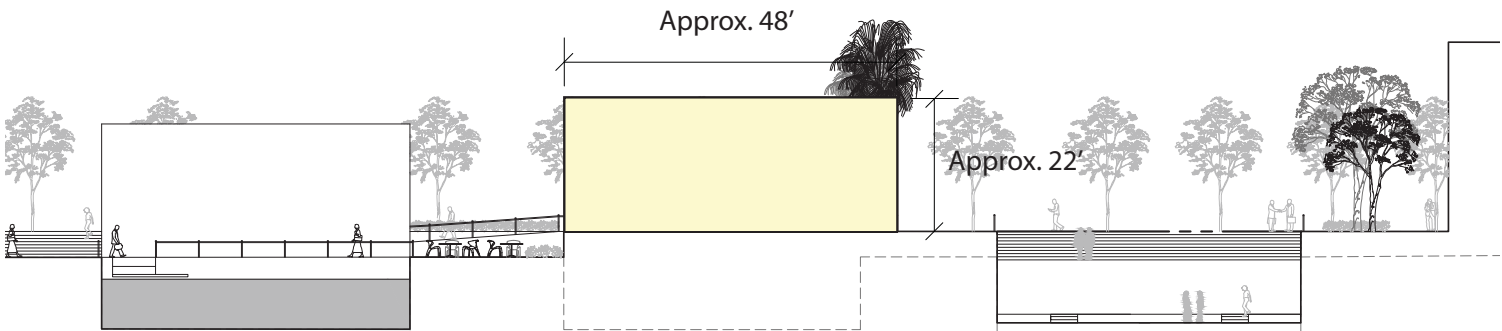
Figure VI.5.b - RESTAURANT / BAR



The restaurant/bar is a fully public amenity for use by the neighborhood as well as the County and City residents living nearby. The space is meant to provide a dining experience for patrons centered around the location of the restaurant and its relationship to the surrounding drydocks and river.

**POSSIBLE AMENITIES:**

- Public Dining Area
- Public Bar
- Outdoor Seating
- Public Restrooms
- Waterfront Views





VI.5 OTHER PUBLIC BENEFITS

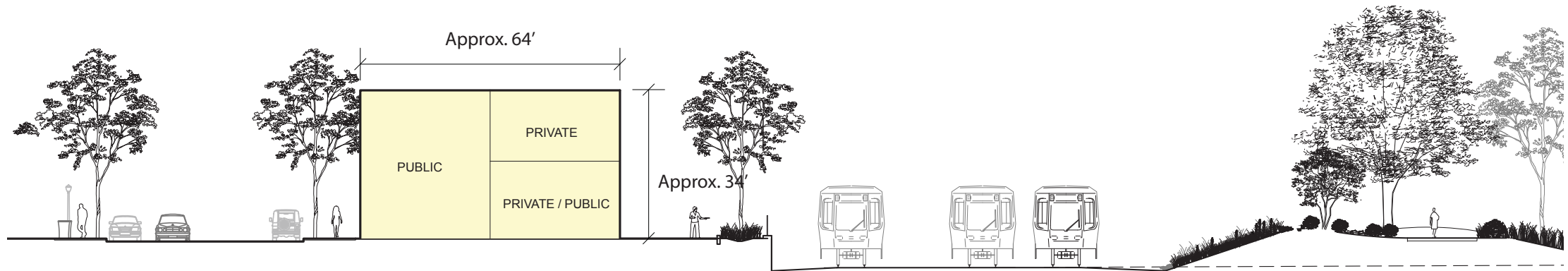
Figure VI.5.c - COMMUNITY FACILITIES SPACE



The Emergency Services/Community Meeting Space is a semi-public facility meant to serve the residents of the immediate neighborhood. The facility acts as a home for the community’s emergency services, providing space for its security, communications, and response needs. The community meeting space allows for public access to the facility and provides an informal multi-purpose space for the residents of the neighborhood to come together for group activities, such as public information meetings, local elections, social support, and other purposes. It is expected that the facility be equipped to handle the two distinct functions which will likely require an internal separation of program, though there may be opportunities to share space between the two, such as entrances, bathrooms, and other support spaces, in order to prevent redundancy.

**POSSIBLE AMENITIES:**

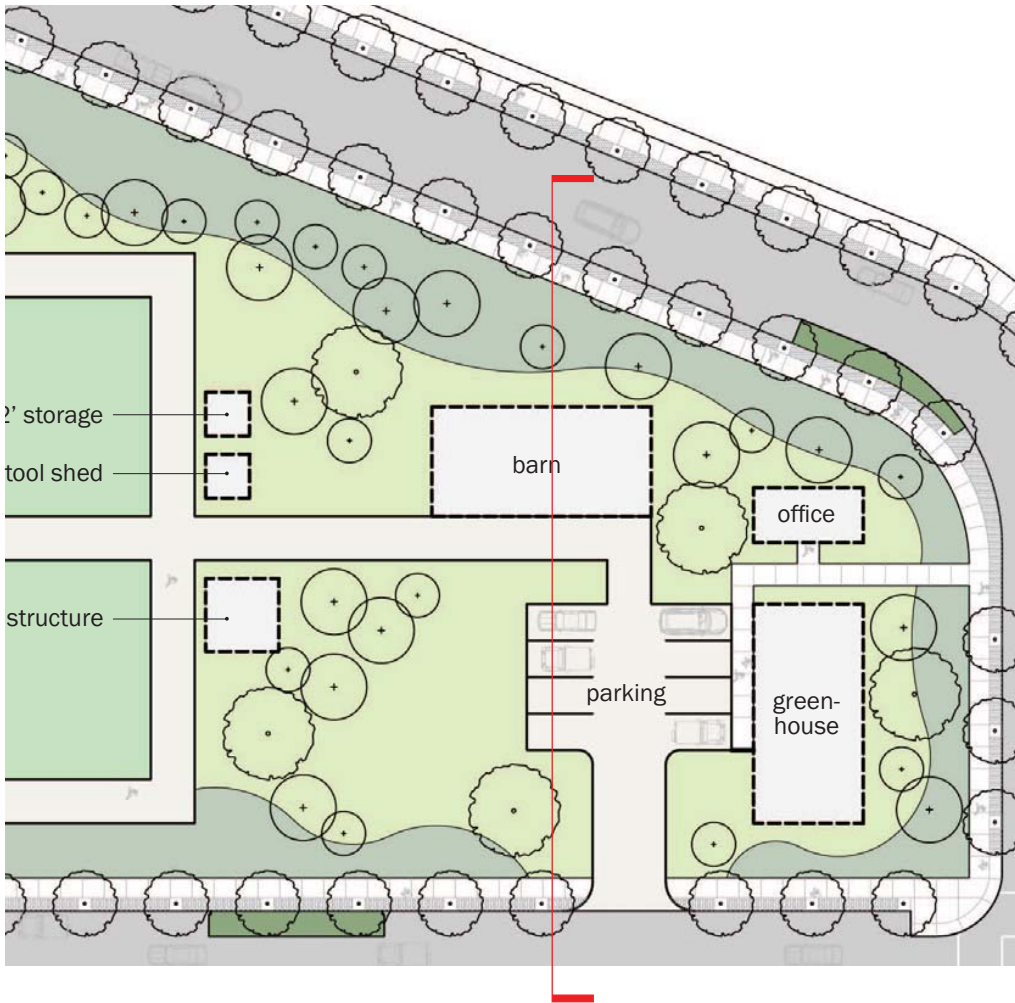
- Multi-Purpose Community Meeting Space
- Public Restrooms
- Community Learning Center
- Emergency Services Security/Communications/Response Spaces





VI.5 OTHER PUBLIC BENEFITS

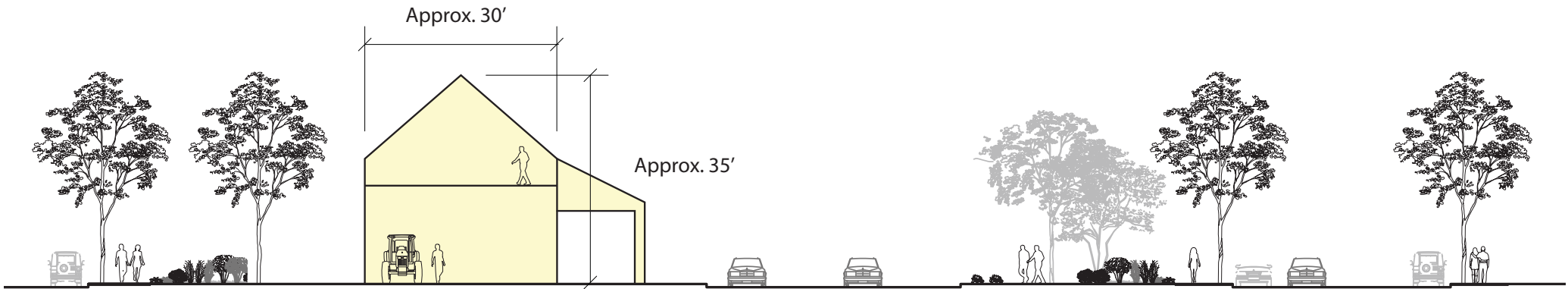
Figure VI.5.d - BARN



The barn is part of the community farm facilities and is a semi public space open to those community members who are involved in the community supported agriculture (CSA) program. The requirements for the barn space are to provide the necessary facilities needed to carry out the day to day farming operations of the CSA program. The barn provides space for the CSA program to store any and all farming equipment, tools, and machinery needed in the production of its crops and produce. The barn also provides space necessary to carry out the distribution and preparation needs of the program. The space needed should be flexible enough to handle a variety of general farming tasks as well as farming-related community events.

**POSSIBLE AMENITIES:**

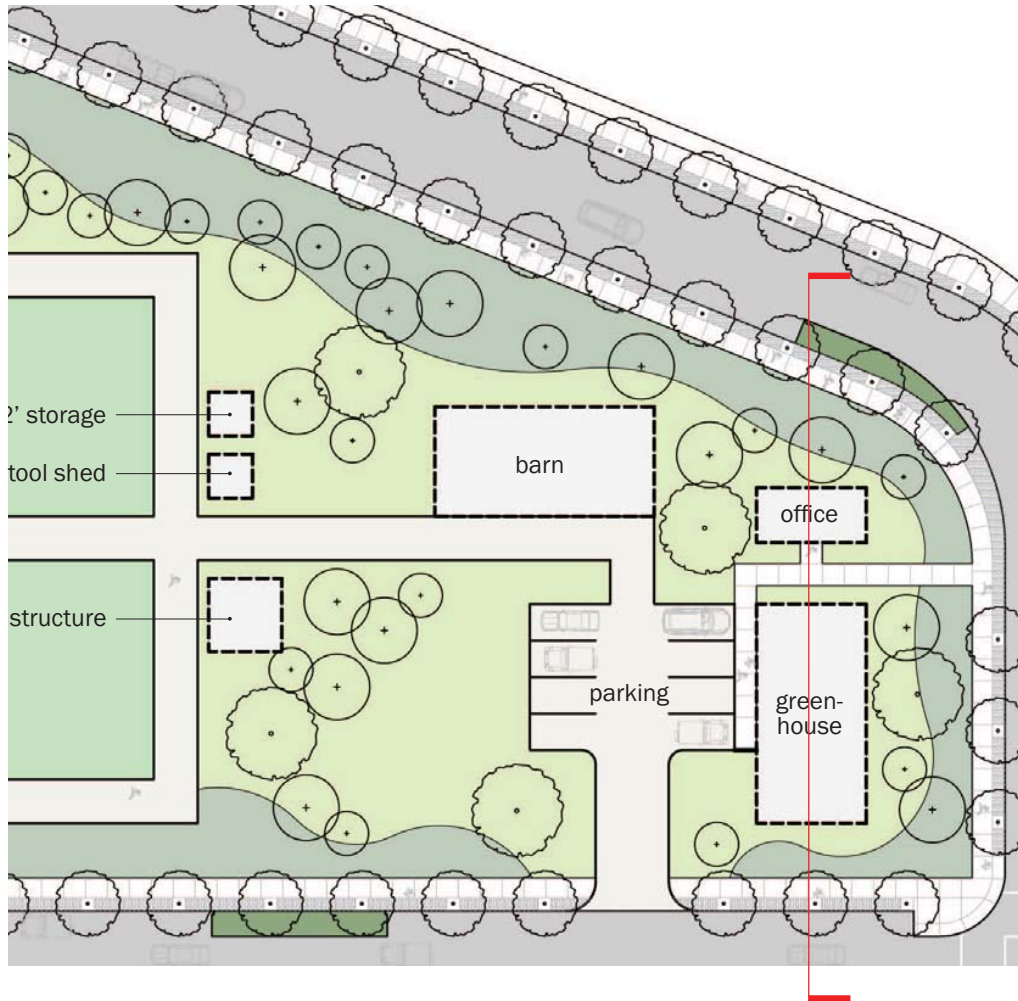
- Drive bay (wide corridor for storage of large farm machinery)
- General tool and equipment storage
- Loft Space
- Possible canning facility
- Possible shipping facility
- General multi-purpose space
- Produce preparation space





VI.5 OTHER PUBLIC BENEFITS

Figure VI.5.e - GREENHOUSE / FARM OFFICE



The greenhouse and office are part of the community farm facilities and are semi public spaces open to those community members who are involved in the community supported agriculture (CSA) program. The requirements for the farm office space are to provide the necessary facilities needed to carry out the day to day managerial operations of the CSA program. The greenhouse facility provides space for the CSA program to grow and maintain it's crop of vegetables and other produce in a controlled manner, especially in the colder months. It is necessary that this space be flexible enough such that heat, light, moisture, and temperature are able to be controlled to optimal growing conditions.

**POSSIBLE AMENITIES:**

- Automatic climate controls including:
  - Mechanical ventilation
  - Operable vents
  - Automatic shading devices
  - Watering systems (sprinklers or drip lines)
- Reconfigurable benches
- Heated ground slab

